



Special City Council Business Meeting Agenda
Monday, May 9, 2016, 7:00 p.m.
City Hall – 705 North Avenue

COUNCIL MEETINGS

City Hall
Council Chambers
1st and 3rd
Thursdays at
6:00 P.M.

Tom Phillips
Mayor

Council Members:
Erika Isley
Ed Kuhl
David Lester
Jaki Livingston
Stephanie Riva

Vacant
City Manager

Jean Furler
Finance Director

Jodi Eddleman
City Clerk

Ryan Coburn
Fire Chief

Greg Staples
Police Chief

Tim Hoskins
Public Works Director

Nancy Kuehl
Parks & Recreation
Director

Wade Wagoner
Planning and
Economic
Development Director

Holly Sealine
Library Director

Jim Dougherty
City Attorney

1. Call to order.
2. Approval of agenda.
3. Presentations - Shull and Company: Audit presentation
4. Welcome of guests and public comment.
(3 minute limit, no action)
5. [Consideration of the first and possibly more readings of proposed ordinance amending the Orchard View PUD.](#)
6. [Consideration of a resolution approving the Norwalk Orchard View townhomes preliminary plat and site plan.](#)
7. [Consideration of second and possible third reading of an ordinance amending the master plan and rules, regulations and guidelines for the Echo Valley Community Planned Unit Development as contained in Ordinance No 03-08.](#)
8. [Resolution approving the preliminary plat for Market Place at Echo Valley](#)
9. [Discussion and possible action regarding request for urban chickens – 1313 Main Street.](#)
10. [Consider request by Herb Eckhouse of LaQuercia for the City to co-sponsor as an economic development business retention event, their front yard party during the American Cheese Festival on July 26, 2016.](#)
11. [Consideration of a resolution amending the Holland Park Farms development agreement](#)
12. [Consideration of a resolution approving FY 16/17 salary schedule – COLA/general wage increase](#)
13. [Consideration of a resolution prohibiting tobacco use in the city parks.](#)
14. [Fire Department reorganization discussion](#)
15. [Receive and file the FY14/15 audit for the City of Norwalk](#)
16. [Consideration of third reading of an ordinance amending the master plan and rules, regulation and guidelines for the Dobson Planned Unit Development as contained in Ordinance No. 15-05](#)
17. Council Inquiries and staff updates.
18. Adjournment

Culture and Recreation Citizen Improvement Advisory Committee

Recommendations

Immediate Need (now - 2 years)

- * Consider renovating the baby pool space to a splash pad.
- * Incorporate additional splash pads in current or future parks as they are developed.

- * Secure 120 acres of land for Signature Park (buy land outright or buy 80 with option on additional 40)
 - This would include 80 acres for the new sports complex and 40 acres for a park.
 - The signature park would include community center and performance pavilion.
 - Keep open dialogue with the school and if it makes sense locate the park in close proximity so the two facilities could share paved parking.

Short-term Need (1 to 3 years)

- * Relocate and expand sports complex.
- * Develop a plan for bringing more public art into Norwalk.

Mid-term Need (4 to 7 years)

- * Additional gym/flex space. (coordinate planning with the school district)

- * Library renovation (storage, outlets, etc.) (carpet replacement is listed in capital plan, but this should include a larger renovation)

Long-term Need (8+ years)

- * New Pool/aquatic center

- * Library expansion

Continuing priorities (should be addressed bit by bit each year)

- * Bike paths - continue to develop bike paths and apply for grant funds to connect bike paths annually.

- * Continue supporting current community events like Jazz in July, Farmer's Market, RunNorwalk, Norwalktoberfest, and 4th of July activities.

Funding recommendations:

Pursue \$0.27 library levy as means of funding the library expansion.



BUSINESS OF THE CITY COUNCIL

AGENDA STATEMENT

Item No. 7 & 8
For Meeting of 05.05.16

REQUEST: Public hearing regarding an amendment to the master plan and rules, regulation and guidelines for the Orchard View Planned Unit Development to change the setbacks, buffer requirements, and ownership requirement of Parcel 3 of the Orchard View Planned Unit Development

Consideration first and possible second and third reading of an ordinance amending the Orchard View Planned Unit Development

STAFF CONTACT: Luke Parris, AICP
City Planner

APPLICANT(S): Norwalk Land Co. LLC
PO Box 267
Johnston, Iowa 50263

LOCATION: Northeast on the intersection of Wright Road and Orchard Hills Drive.

CURRENT USE: The site is currently vacant development ground.

PROPOSED USE: The proposal does not change the uses but requests the following:

- Setbacks from private roadways are identified on the attached Master Plan for Parcel 3.
- Change the required setback for the complex from 30' to 35' and to allow for a 15' buffer to overlap the 35' setback.
- The requirement for "owner occupied units" be deleted.

The Master Plan is included as Attachment A.

ZONING HISTORY: The site was previously part of the Orchard Hills PUD and was zoned as single-family residential. A new PUD was developed and the site was zoned as Parcel 3 of the Orchard View PUD in 2012 (Ordinance 12-09). At the time of the PUD development there was a lot of involvement of the surrounding neighborhood regarding the uses that would be allowed in Orchard View. Ultimately a PUD was adopted that included single family uses and the R-3 Parcel 3, which included a restriction that any units be owner-occupied. Since approval of the PUD, ownership of the property has changed hands multiple times. Included as an attachment are the minutes from the previous public hearing for the original Orchard View PUD.

LAND USE PLAN:	The future land use plan identifies the area as High Density Residential. This land use classification identifies multi-family dwellings as a typical use.
SURROUNDING LAND USE PLAN AND ZONING:	<p>Surrounding land use planned for the area is:</p> <ul style="list-style-type: none"> • North, East, and West – Medium Density Residential. • South – Park/Recreation <p>Surrounding zoning for the area is:</p> <ul style="list-style-type: none"> • North, East, and West – R-1 Residential in the Orchard View and Orchard Hills PUD. • South – unincorporated ground not zoned.
FLOOD INFORMATION:	The proposed development is not located in a floodplain.
MAJOR STREET PLAN/TRAFFIC:	The request to amend the Orchard View Planned Unit Development does not change the expected impact on traffic in the area from a townhome development.
DEVELOPMENT SECTOR ANALYSIS:	Parcel 3 is located on the west side of the proposed Orchard Hills Drive connection to Wright Road. Surrounding development ground is owned by Norwalk Land Co and each piece is in varying stages of the development process.
STAFF ANALYSIS:	<p>The future land use plan for the area is identified as High Density Residential. The PUD currently calls for R-3 zoning in this area. The proposed amendment does not request a change in zoning district. The proposal requests:</p> <ol style="list-style-type: none"> 1. Setbacks from private roadways are identified on the attached Master Plan for Parcel 3. 2. Change the required setback for the complex from 30' to 35' and to allow for a 15' buffer to overlap the 35' setback. 3. The requirement for "owner occupied units" be deleted. <p>For request #1, the PUD currently states that buildings shall be setback 25' from any private roadway. This is mainly to provide for adequate driveway length to avoid a car parked in a driveway from hanging out into the roadway. When siting the buildings, the developer found that most buildings could be setback 25' from the street. However, at some intersections, it was not feasible to maintain the 25' separation on the side of a unit. To give assurances that the majority of buildings would maintain a 25' separation, the developer has requested that the setback from private roadways be identified on the attached site plan for the development. This would lock in the building layout and assures the City that the driveways for each unit will be of appropriate length.</p> <p>For request #2, the PUD currently requires a 30' setback and a landscaped buffer. The City Subdivision Ordinance requires that a buffer be in addition to a required setback. The developer had proposed a 15' wide buffer with appropriate trees and shrubs. The combination of the 15' buffer and the 30' setback meant that the true building setback was 45' from the external lot lines of the complex. The developer requests that they increase the setback to 35' and allow the 15' landscaped buffer to overlap the setback. The developer's other option would be to construct a masonry</p>

buffer wall and adhere to the 30' setback. The developer and staff both agreed a buffer wall would not be the best option for a residential setting. Staff contacted other metropolitan area communities to learn if they allowed buffers to overlap. Below are the results:

- Ankeny – no current buffer requirement, negotiated with each development, when provided they are allowed to overlap
- Ames – allowed to overlap
- Des Moines – allowed to overlap
- Johnston – allowed to overlap
- Pleasant Hill – allowed to overlap
- Waukee – allowed to overlap
- West Des Moines – allowed to overlap except for double frontage lots

For request #3, the current PUD requires that only owner occupied lots be developed on Parcel 3. The developer requests that this requirement be deleted from the PUD as they would like the option to rent some of the units. Staff is concerned about the legality of the current PUD language. Additionally, standard R-3 zoning would not preclude a developer from renting units. Staff also contacted the planning departments of other metro communities and inquired if they regulate the occupancy type of a development or zoning district. The response was that regulating occupancy type was not land use and should not be regulated by zoning. Included as an attachment to this report are the direct responses that staff received from other communities.

Norwalk Land Co. currently owns the majority of the surrounding ground that they are developing into single family lots. This townhome project should be completed before any adjacent lots are developed. This would mean that future owners of any adjacent single-family homes would be aware of the development.

**STAFF
RECOMMENDATION:**

Staff recommends approval of the amendment to Parcel 3 of the Orchard View Planned Unit Development. The proposed amendments are relatively minor and do not alter the intent of that the original PUD had for Parcel 3. The proposal further locks in the layout of the townhome development through the inclusion of the Master Plan, providing further assurances on the type of development to occur on Parcel 3.

**PLANNING AND ZONING
ACTION:**

The Planning and Zoning Commission recommends approval of the proposed amendment as it was presented:

- Setbacks from private roadways are identified on the attached Master Plan for Parcel 3.
- Change the required setback for the complex from 30' to 35' and to allow for a 15' buffer to overlap the 35' setback.
- The requirement for "owner occupied units" be deleted.

ATTACHMENTS:

- Attachment "A" – Orchard View PUD Parcel 3 Norwalk Orchard View Townhomes Master Plan
- Attachment "B" – Norwalk Orchard View Townhomes Vicinity Map
- Attachment "C" – Responses to Regulating Occupancy Type
- Attachment "D" – Planning & Zoning Commission Minutes – 8-8-2012
- Attachment "E" – Planning & Zoning Commission Minutes – 8-22-2012
- Attachment "F" – Comprehensive Plan Map

<p>____ Resolution <u> X </u> Ordinance ____ Contract ____ Other (Specify) _____</p> <p>Funding Source: <u> NA </u></p> <p style="text-align: center;"></p> <p>APPROVED FOR SUBMITTAL:</p> <p style="text-align: center;">Planning & Economic Development Director</p>
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ORDINANCE NO. _____

AN ORDINANCE AMENDING THE MASTER PLAN AND RULES, REGULATION, AND GUIDELINES FOR THE ORCHARD VIEW PLANNED UNIT DEVELOPMENT AS CONTAINED IN ORDINANCE NO. 12-09

BE IT ENACTED BY THE CITY COUNCIL OF THE CITY OF NORWALK, IOWA:

SECTION 1. PURPOSE. The purpose of this ordinance is to amend the master plan and rules, regulation, and guidelines for Orchard View Planned Unit Development as contained in Ordinance No. 12-09.

SECTION 2. AMENDMENT. The Orchard View Planned Unit Development is hereby amended with the following Master Plan, additional language (highlighted), and deleted language (red strike-through):

Orchard View Planned Unit Development Parcel 3 Norwalk Orchard View Townhomes Master Plan Map: The Master Plan included as Attachment "A" is hereby amended into the Orchard View Planned Unit Development for Parcel 3.

*Townhouses - a townhouse, wherein the owner of the dwelling unit owns the lot beneath the dwelling unit, shall be permitted in Parcel 3 provided the lot for one dwelling has a minimum area of six thousand two hundred fifty (6,250) square feet, minimum width of fifty (50) feet and minimum side yard setback of zero (0) feet at the side lot line where the dwellings are attached. Public street frontage shall not be required for townhouse lots which are part of a complex which does not require a public street as part of the City's transportation network and is master planned with a private common roadway serving the townhouse lots and maintained by an association of townhouse homeowners. A private, common roadway serving a complex of townhouse dwellings shall not be greater than 660 feet in length for a cul-de-sac and 1,320 feet for a through street, which shall be intended to serve only dwellings within the complex. Individual townhouse lots shall not have minimum setback, lot width and area requirements, provided the tract of land encompassing the townhouse lots and common areas has public street frontage; a minimum width of one hundred (100) feet; a minimum area of forty thousand (40,000) square feet; maximum density of five (8) dwelling units per acre, minimum separation of fifteen (15) feet between residential buildings; ~~minimum separation of twenty five (25) feet between a residential building and common private roadways; and a minimum building setback of thirty (30) feet from all boundaries of the complex, including public streets.~~ The residential buildings shall maintain a separation from the common private roadway as identified on the attached Master Plan for Parcel 3 (Attachment "A"). The minimum building setback shall be 35' from all boundaries of the complex, including public streets. The development of a townhouse complex shall require the approval of a site plan in accordance with the provisions of Chapter 17.80, Site Plans, and approval of Homeowners Association documents by the City which establishes provisions for maintenance of common areas.

SPECIFIC INFORMATION NOT IN TABLES

PARCEL 3. This Parcel may be no greater than 10 acres MIL. Proposed multi -family structures along all the perimeters shall be no taller than two (2) stories. ~~A 30' landscape buffer shall be provided in addition to the required setback on any portion bordering single family residential zoning.~~ A 15' landscaped buffer shall be provided in accordance with the buffer regulations for a 15' Buffer 1 in section 17.50.030 of the Zoning Ordinance, including the 1.4 plant multiplier. The 15' landscaped buffer shall be allowed to overlap with the 35' minimum building setback. ~~In addition, the development of this parcel may only include owner occupied units.~~

SECTION 3. SEVERABILITY CLAUSE. In any section, provision, or part of this ordinance shall be adjudged invalid or unconstitutional such adjudication shall not affect the validity of the ordinance as a whole or any section, provision or part thereof not adjudged invalid or unconstitutional.

SECTION 4. EFFECTIVE DATE. This Ordinance shall be in full force and effect after its passage, approval and publication as provided by law.

Passed and approved by the City Council of the City of Norwalk, Iowa on the ____ day of _____, 2016.

Tom Phillips, Mayor

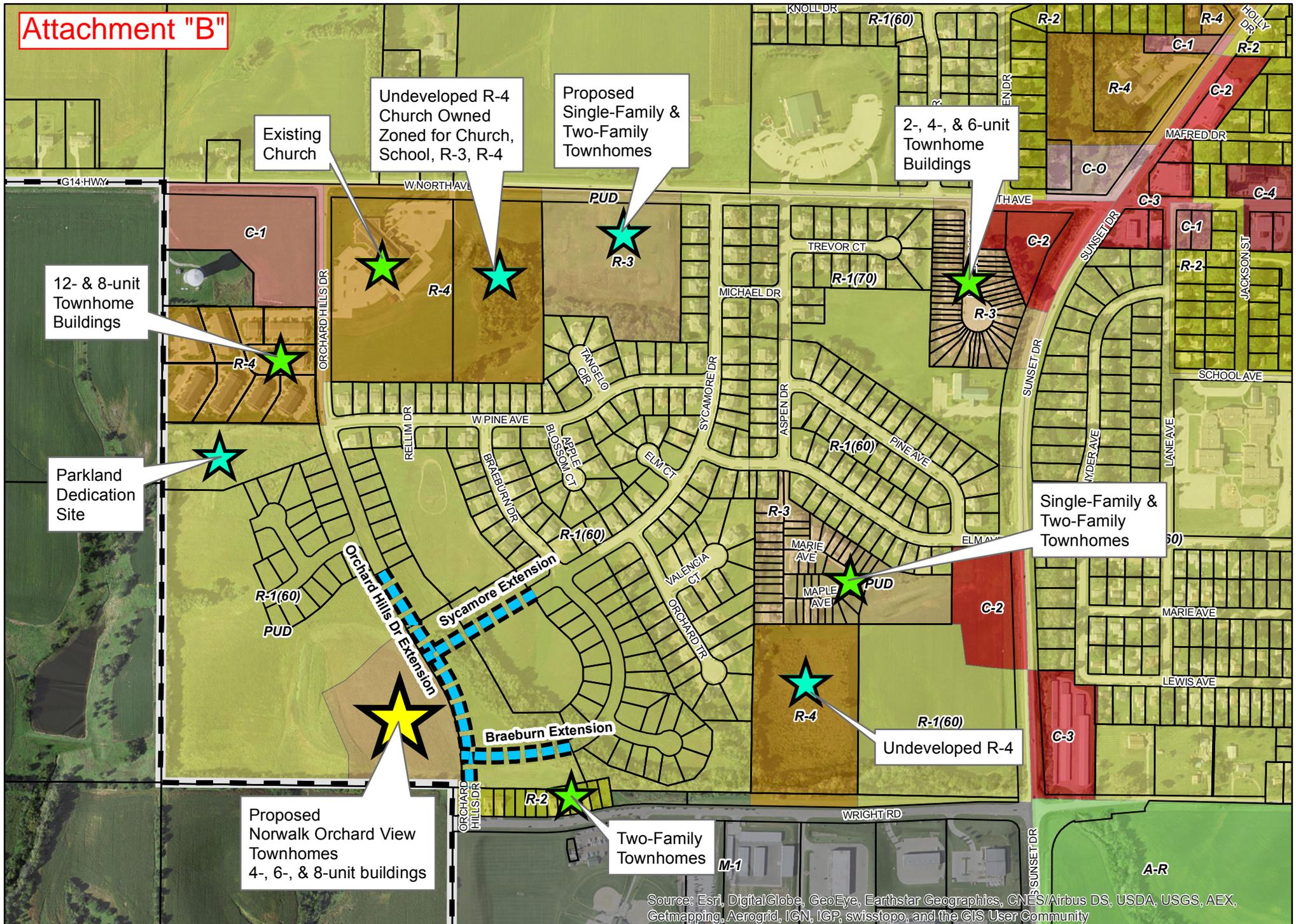
ATTEST:

Jodi Eddleman, Deputy City Clerk

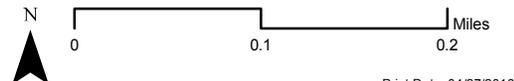
PREPARED BY: Luke Parris, City Planner

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Isley	___	___
Kuhl	___	___
Lester	___	___
Livingston	___	___
Riva	___	___

Attachment "B"



Norwalk Orchard View Townhomes Location



Attachment "C"

RESPONSES TO REGULATING OCCUPANCY TYPE (OWNER VS RENTAL)

Luke-

Believe it or not we actually have a separate zoning districts for owner-occupied multi-family (presumably condos) and rental multi-family (apartments). I have always questioned this though because of all the reasons others have raised. We are currently re-writing our Zoning Code and I am removing one district and not restricting the occupancy type in the other.

Thanks,

Brad Deets, Development Services Director City of Waukee
230 W. Hickman Road, Waukee, IA 50263
O: 515-978-7899 | M: 515-250-7986
bdeets@Waukee.org | Waukee.org

Luke,

The city does not regulate type of occupancy.

Kara Tragesser
City of West Des Moines

I believe it could result in a Fair Housing Act claim and should be avoided especially if you are receiving federal funds from HUD or other agencies. Des Moines has been very clear that the method of tenancy/occupancy is not a land use.

Mike Ludwig
Planning Administrator
City of Des Moines

Urbandale has dealt with this recently. Our position is that the City cannot require development to be owner occupied, and therefore no such language in any code or master plan.

Steven S. Franklin, APA, PLA
Community Development Director
City of Urbandale
3600 86th Street
Urbandale, Iowa 50322
515-331-6720
sfranklin@urbandale.org

Ankeny does not regulate rental vs owner occupied.

Eric Jensen
Planning & Building Director
City of Ankeny

I'll let your city attorney put the final stamp on that. My two cents is that putting it in a PUD is problematic, possibly giving rise to Equal Protection claims.

Gary Taylor, J.D., AICP
Interim Director, Community & Economic Development Program
Iowa State University Extension & Outreach
Associate Professor, Community & Regional Planning
2321 North Loop Drive, Suite 121
Ames, IA 50010
gtaylor@iastate.edu
Ph: 515.294.8397

Attachment "D"

REGULAR NORWALK PLANNING & ZONING MEETING 08-08-2012

A regular meeting of the Norwalk Planning and Zoning Commission was held at the Norwalk City Hall, 705 North Avenue, Wednesday, August 8, 2012. The meeting was called to order at 5:45 P.M. by Chairperson Stephanie Riva. Those present at roll call were John Fraser, Melissa Hill, Dan Schulz, Rodney Martinez, Kim Leonhardt and Stephanie Riva. Absent: Jim Huse.

Staff present: Mike Johnson, City Planner/Interim Development Services Director and Shelley Heisdorffer, Development Services Assistant. Frank Curtis, City Council was also present.

Huse arrived at 5:46 P.M.

12-40 Motion by Schulz and seconded by Martinez to approve the agenda with the addition of New Business Item C: Resolution Establishing A Temporary Moratorium on the Consideration and Approval of Any Rezoning Hearings or Comprehensive Land Use Plan Amendments Pending the Completion of the New Comprehensive Plan. Approved 7-0.

12-41 Motion by Huse and seconded by Leonhardt to approve the minutes from the July 25, 2012 regular meeting. Approved 7-0.

Chairperson Riva welcomed the guests present and asked if anyone wished to speak on a topic that was not on the agenda. With no guests wishing to speak, the business portion of the meeting was opened.

The first item on the Agenda was Proposed Land Use Amendment – Orchard Hills Drive (R-1 to PUD). Mr. Johnson informed the Commission that he received a letter from a resident who wanted it to be entered into record that they were in opposition of the proposed land use amendment. Mr. Johnson gave each of the Commission members a copy of that letter. Mr. Johnson informed the Commission that he and Marketa Oliver, City Manager have met with Mr. Gillotti who is representing Road Contractors (the applicant). They informed him of the issues that many of the residents have with the R-3 portion of his request. Mr. Gillotti agreed to modify the PUD to require the 10 acre R-3 parcel to the far southeaster corner of the property. Staff thought that would be a more suitable location for the higher density and will provide a good buffer to the sports complex. All of the previously included conditions still apply to the development of the R-3 parcel if approved.

Steve Gillotti, Road Contractors, explained that they have owned this land for over 30 years. The land was optioned to a developer who didn't move on it and they now have the land back. Road Contractors now desires a PUD that stands on its own. They have no intentions of changing the land use, they would just like to be able to control their own destiny. They would like to see the zoning through and then will sell to a developer who will have to abide by the PUD. Mr. Gillotti has a long history with Norwalk and just wanted to make sure the zoning process was handled appropriately with the City.

Chad Ross, 518 West Pine Avenue spoke in opposition of the proposed land use amendment. He was informed by the City that the area around his home would be single family. He also has a concern that it looks like the park has shrunk from the original plans.

William Brown, 605 West Pine Avenue also spoke in opposition of the proposed land use amendment. He called City Hall prior to moving into his home and was informed that the areas around his home were zoned R-1.

Craig King who is a representative of Mr. Gillotti and Road Contractors spoke to the audience. He explained there are different kinds of medium density and what they are looking at would be a detached cottage with a home owners association. Older people and young professionals are very attracted to these types of homes due to the association taking care of some maintenance.

These areas keep a residential feel but the developer is able to make the most of their land. This adds variety along with quality homes to the area.

Mr. King also explained that because they would be removing this land from a current PUD and would be creating their own, that also changes the park requirements. The park in the old PUD would not have to be quite as large, but there would also be a requirement for a park in the new PUD.

Mr. King suggested that the Commission put requirements on the new PUD to restrict the homes built there to be association homes only.

Richard White, 619 West Pine Avenue questioned our master plan and which one is currently being used. Mr. Johnson informed him that we are currently using the 2005 Comprehensive Plan but that even though things are set, there is always a chance that through a process such as this, things can be changed.

Mr. Gillotti informed the Commission that he would be fine with them putting density restrictions on the PUD. Mr. Gillotti requested that they at least divide the land off into its own PUD tonight and then work out the details and restrictions of the PUD at the next meeting.

Mr. Johnson informed Mr. Gillotti and the Commission that he was not comfortable doing that and that he would prefer to do it all at one time so as not to create more of a mess. The Commission discussed holding a special meeting to take care of the PUD request.

12-42 Motion by Hill and seconded by Schulz to table Proposed Land Use Amendment – Orchard Hills Drive (R-1 to PUD). Approved 7-0.

The next item on the Agenda was Rezoning Petition 38.38 Acre – Orchard Hills Planned Unit Development to Orchard View Planned Unit Development.

12-43 Motion by Martinez and seconded by Fraser to retable Rezoning Petition 38.38 Acre – Orchard Hills Planned Unit Development to Orchard View Planned Unit Development. Approved 7-0.

The next item on the Agenda was Resolution Establishing a Temporary Moratorium on the Consideration and Approval of Any Rezoning Hearings or Comprehensive Land Use Plan Amendments Pending the Completion of the New Comprehensive Plan. Mr. Johnson explained that due to the items that have been on the Agenda recently, he and Ms. Oliver feel it would be a good idea to put a moratorium on any rezoning or land use amendment until the new Comprehensive Plan is complete.

12-44 Motion by Leonhardt and seconded by Martinez to approve Resolution Establishing a Temporary Moratorium on the Consideration and Approval of Any Rezoning Hearings or Comprehensive Land Use Plan Amendments Pending the Completion of the New Comprehensive Plan. Approved 7-0.

The next item on the Agenda was a study session of Comprehensive Plan review. Mr. Johnson asked the Commission to study the materials for the work session that is scheduled for August 13, 2012.

12-45 Motion by Huse and seconded by Hill to adjourn meeting at 6:51 p.m. Approved 7-0.

Stephanie Riva, Chairperson

Mike Johnson, City Planner/Interim
Development Services Director

Attachment "E"

REGULAR NORWALK PLANNING & ZONING MEETING 08-22-2012

A regular meeting of the Norwalk Planning and Zoning Commission was held at the Norwalk City Hall, 705 North Avenue, Wednesday, August 22, 2012. The meeting was called to order at 5:45 P.M. by Chairperson Stephanie Riva. Those present at roll call were John Fraser, Melissa Hill, Dan Schulz, Rodney Martinez and Stephanie Riva. Absent: Jim Huse and Kim Leonhardt.

Staff present: Mike Johnson, City Planner/Interim Development Services Director and Shelley Heisdorffer, Development Services Assistant.

12-46 Motion by Martinez and seconded by Fraser to approve the agenda as presented. Approved 5-0.

12-47 Motion by Hill and seconded by Schulz to approve the minutes from the August 8, 2012 regular meeting. Approved 5-0.

Chairperson Riva welcomed the guests present and asked if anyone wished to speak on a topic that was not on the agenda. With no guests wishing to speak, the business portion of the meeting was opened.

The first item on the Agenda was Proposed Land Use Amendment – Orchard Hills Drive (R-1 to PUD).

12-48 Motion by Martinez and seconded by Fraser to untable Proposed Land Use Plan – Orchard Hills Drive (R-1 to PUD). Approved 5-0.

Mr. Johnson informed the Commission that after the last Planning and Zoning meeting, staff revised the proposed Orchard View PUD to accommodate the concerns discussed during the hearing. The new PUD decreases the density per acre from 10 dwelling units per acre to 8 dwelling units per acre. The revised PUD also provides for regulations which encourage detached patio townhomes. All of the previously included conditions still apply. Mr. Johnson noted that this provides a nice transition from the sports complex.

12-49 Motion by Hill and seconded by Martinez to approve Proposed Land Use Amendment – Orchard Hills Drive (R-1 to PUD). Approved 5-0.

The next item on the Agenda was Rezoning Petition 38.38 Acre – Orchard Hills Planned Unit Development to Orchard View Planned Unit Development.

12-50 Motion by Hill and seconded by Schulz to untable Rezoning Petition 38.38 Acre – Orchard Hills Planned Unit Development to Orchard View Planned Unit Development. Approved 5-0.

Mr. Johnson explained to the Commission that although this would change the zoning, a master plan would still have to come before the Commission by the developer and at that time the Commission would be able to put conditions on the plan.

Chad Ross, 518 West Pine Ave., explained that staff had addressed his concerns from the last meeting and he is no longer opposing the rezoning. He would, however, like the neighborhood to be notified when the developer brings plans in to develop the area.

12-51 Motion by Martinez and seconded by Schulz to approve Rezoning Petition 38.38 Acre – Orchard Hills Planned Unit Development to Orchard View Planned Unit Development. Approved 5-0.

The next item on the Agenda was new business item Proposed Accessory Structure (27' x 26') – St. John's Catholic Church, 720 Orchard Hills Drive. Mr. Johnson reported that St. John's

requested approval of an accessory structure to be built on its property at 720 Orchard Hills Drive. The structure as proposed would be 27' x 26' and be located immediately adjacent to the existing dumpster enclosure on the south side of the parking lot. The structure with approval would provide for additional storage space and be used for storage of grounds maintenance equipment.

Code requires that accessory structures be at least 3 feet from side property lines and at least 5 feet from rear property lines. The proposed structure is consistent with all setback requirements. Architecturally, the structure will be consistent with the design of existing church.

Mr. Johnson explained that staff would recommend approval of the structure with the condition that it be constructed of similar materials and be similar architecturally to the principal structure.

Roger Joanning, 521 West Pine Ave., asked where the garage would be located on the property. Mr. Johnson showed him a map.

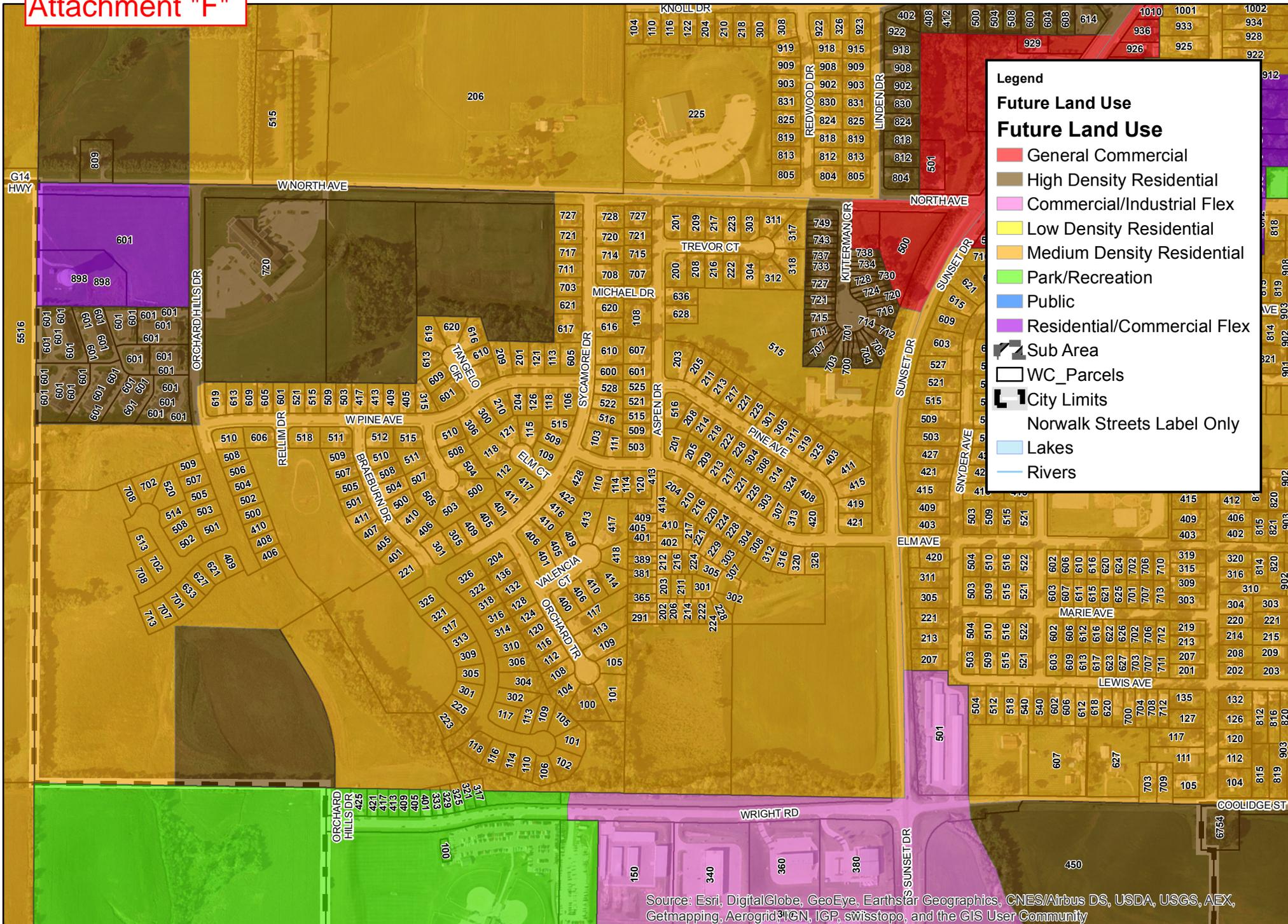
12-52 Motion by Fraser and seconded by Hill to approve Proposed Accessory Structure (27' x 26') – St. John's Catholic Church, 720 Orchard Hills Drive with the condition that it be constructed of similar materials and be similar architecturally to the principal structure. Approved 5-0.

The next item on the Agenda was a study session on Comprehensive Plan progress update and disbursement of second half of first draft. Mr. Johnson asked the Commission to study the materials and a work session would be scheduled for the next week or two.

12-53 Motion by Martinez and seconded by Fraser to adjourn meeting at 6:05 p.m. Approved 5-0.

Stephanie Riva, Chairperson

Mike Johnson, City Planner/Interim
Development Services Director



Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 9
For Meeting of 5.5.2016

- REQUEST:** Request from Norwalk Land Co to approve the Preliminary Plat and Site Plan of the Norwalk Orchard View Townhomes
- STAFF CONTACT:** Luke Parris, AICP
City Planner
- APPLICANT(S):** Norwalk Land Co. LLC
PO Box 267
Johnston, Iowa 50263
- GENERAL DESCRIPTION:** This request would create 76 townhome lots and site plan out the location of buildings and private roadways on Parcel 3 of the Orchard View PUD. The proposal is developed with the requests submitted in a separate PUD amendment.
- IMPACT ON NEIGHBORHOOD:** The surrounding ground is currently undeveloped with the exception of nearby two-family homes along Wright Road to the southeast. The surrounding development is all planned as single family residential as a part of various PUDs.
- VEHICULAR & PEDESTRIAN TRAFFIC:** The plat/site plan shows two connections onto Orchard Hills Drive at the intersections of Sycamore Drive and Braeburn Drive. The streets internal to the development are private and meet the city's standards for private streets. A four-way controlled intersection with stop signs will likely be required at the Sycamore Drive intersection.
- TRAIL PLAN:** The site would have a 5' wide sidewalk installed along Orchard Hills Drive. There are no sidewalks identified. They are not required per code. Staff was concerned that sidewalks internal to the development would encourage parking on said sidewalks and felt that the proximity to the sidewalk and trails along Orchard Hills Drive was close enough to not request sidewalks internal to the development.
- ZONING HISTORY FOR SITE AND IMMEDIATE VICINITY:** The site was previously part of the Orchard Hills PUD and was zoned as single-family residential. A new PUD was developed and the site was zoned as Parcel 3 of the Orchard View PUD in 2012 (Ordinance

12-09). At the time of the PUD development there was a lot of involvement of the surrounding neighborhood regarding the uses that would be allowed in Orchard View. Ultimately a PUD was adopted that included single family uses and the R-3 Parcel 3, which included a restriction that any units be owner-occupied. Since approval of the PUD, ownership of the property has changed hands multiple times.

A request has been submitted to amend the following conditions in the PUD:

- Setbacks from private roadways are identified on the attached Master Plan for Parcel 3.
- Change the required setback for the complex from 30' to 35' and to allow for a 15' buffer to overlap the 35' setback.
- The requirement for "owner occupied units" be deleted.

The surrounding ground is zoned R-1 in the Orchard View and Orchard Hills PUDs. The ground directly south of the proposed development is not located in the City and is not zoned.

**BUFFERS REQUIRED/
NEEDED:**

The proposed development would require a buffer next to any adjacent single-family districts. Single family districts are located on the east, west, and north. The developer has shown a 15' wide buffer that includes the required 1.4 planting multiplier identified in the City Zoning Ordinance. The developer has requested that the buffer be allowed to overlap the setback in a proposed PUD amendment. Staff reviewed the buffer requirements of surrounding metro communities and found that all would allow a buffer to overlap the setback.

DRAINAGE:

The development includes a storm sewer system that connects with the City system along Orchard Hills Drive. The western portion of the development drains to the southwest into a detention pond that releases at the south property line.

DEVELOPMENT HISTORY:

The area was planned as a PUD on October 3, 2012. A preliminary plat that included the area as an outlot was approved in January 2014.

FLOODPLAIN:

None of the proposed lots are located within a floodplain.

PARKLAND:

Parkland dedication was identified as a park in the northern area of the Orchard View development.

**OPEN SPACE AND
LANDSCAPING:**

The Zoning Ordinance requires 30% open space. The development is 294,489 square feet and would be required to have 88,347 square feet. The Developer has provided 143,251 (48.6%) square feet of open space. The Developer has provided 59 trees and 89 shrubs are to meet the planting requirements. An additional 215 trees and 374 shrubs are provided as part of the required landscaped buffer.

PARKING REQUIREMENTS: The zoning ordinance requires 2.5 spaces for each dwelling unit. One garage space can be counted towards the requirement, as well as driveway space. The development has 76 units and would be required to have 190 parking spaces. Each unit has an attached garage and room for two cars parked in a driveway. This provides 228 parking spaces. An additional 8 visitor parking spaces have been provided.

ARCHITECTURAL STANDARDS: The City's Architectural Standards require that multi-family townhomes incorporate 3 different materials from the City's list of classified materials. The developer submitted elevations for units that they would like to build and they meet the City's Architectural Standards.

UTILITIES: WATER, SANITARY SEWER, STORM SEWER.

- An 8" public water main is provided throughout the site with an associated easement.
- Hydrants are shown throughout the site at appropriate spacing to provide adequate coverage to all units.
- An 8" public sanitary sewer is to be constructed along Braeburn Drive and the proper easements have been shown.
- Private storm sewer is provided throughout the site. A portion of the storm sewer connects into existing City storm sewer, the remaining sewer drains into a private detention basin in the southwest of the development.

RELATIONSHIP TO COMPREHENSIVE LAND USE PLAN: The future land use plan for the area is identified as High Density Residential. This R-3 development meets the intent of the Comprehensive Plan.

STAFF ANALYSIS – ZONING ORDINANCE: The Preliminary Plat consists of 76 townhome lots. The plat consists of 6.77 acres of land northwest of the proposed intersection of Orchard Hills Drive and Wright Road.

Streets shown will be private and maintained by the owner's association. The Zoning Ordinance requires that these streets be signed with blue signs and labeled as "Private Streets."

The area is currently being considered for a rezoning amendment to the Orchard View PUD. The proposed PUD amendment proposes the following changes:

- Setbacks from private roadways are identified on the attached Master Plan for Parcel 3.
- Change the required setback for the complex from 30' to 35' and to allow for a 15' buffer to overlap the 35' setback.
- The requirement for "owner occupied units" be deleted.

The PUD requires that multi-family structures along the perimeter of the development be no taller than two stories. The proposed development includes two-story units along the perimeter and three-story units on the interior of the site.

**PLANNING AND ZONING
RECOMMENDATION:**

The Planning and Zoning Commission recommends that the request for the Preliminary Plat & Site Plan for the Norwalk Orchard View Townhomes be approved with the following conditions:

- That the details of the amendment to the Orchard View PUD be incorporated into the Preliminary Plat.
- That the finalized elevations for the dwelling unit types conform to the City's Architectural Standards.
- That the applicant provides all supporting documentation required within the Norwalk Subdivision Regulations.
- That any significant modifications to the final plat be reviewed and approved by the Planning & Zoning Commission and City Council.

ATTACHMENTS:

Attachment "A" – Norwalk Orchard View Townhomes Preliminary Plat & Site Plan
Attachment "B" – Norwalk Orchard View Townhomes Vicinity Map
Attachment "C" – Norwalk Orchard View Townhomes Elevations

<input checked="" type="checkbox"/> Resolution _____ Ordinance ____ Contract _____ Other (Specify) _____
Funding Source: _____ NA _____

APPROVED FOR SUBMITTAL: Planning & Economic Development Director

RESOLUTION NO. ____

A RESOLUTION APPROVING THE NORWALK ORCHARD VIEW TOWNHOMES PRELIMINARY PLAT & SITE PLAN

WHEREAS, the Planning & Zoning Commission reviewed this request at their regular meeting on March 28, 2016 and recommends approval of the Preliminary Plat & Site Plan; and

WHEREAS, that the details of the amendment to the Orchard View PUD be incorporated into the Preliminary Plat and Site Plan; and

WHEREAS, that the finalized elevations for the dwelling unit types conform to the City's Architectural Standards.

WHEREAS, that the applicant provides all supporting documentation required within the Norwalk Subdivision Regulations; and

WHEREAS, that any significant modifications to the plat be reviewed and approved by the Planning & Zoning Commission and City Council; and

NOW, THEREFORE, BE IT RESOLVED: That the City Council does hereby approve the Preliminary Plat and Site Plan for the Norwalk Orchard View Townhomes as described and shown in Attachment "A" & "C" attached hereto and made a part thereof by reference.

PASSED AND APPROVED this 5th day of May, 2016.

Tom Phillips - Mayor

ATTEST:

JODI EDDLEMAN, CITY CLERK

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Kuhl	___	___
Lester	___	___
Isley	___	___
Riva	___	___
Livingston	___	___

PRELIMINARY PLAT - SITE PLAN

NORNALK ORCHARD VIEW TOWNHOMES

NORNALK, IA

NORNALK LAND CO. L.L.C., P.O. BOX 267, JOHNSTON, IOWA 50263 ATTN: JOHN LARSON



VICINITY SKETCH NORTH NOT TO SCALE

SHEET INDEX

SHEET #	SHEET TITLE
1	COVER
2	DIMENSION PLAN
3	UTILITY PLAN
4	GRADING PLAN
5	LANDSCAPE PLAN

UTILITY CONTACTS:

SANITARY SEWER	- NORNALK PUBLIC WORKS DEPARTMENT (515-481-0228)
WATER MAIN	- NORNALK PUBLIC WORKS DEPARTMENT (515-481-0228)
STORM SEWER	- NORNALK PUBLIC WORKS DEPARTMENT (515-481-0228)
NATURAL GAS UTILITY	- MID AMERICAN ENERGY (515-252-6547)
ELECTRIC	- MID AMERICAN ENERGY (515-252-6547)

AREAS:

BUILDINGS	=	66,647 S.F.	22.7%
IMPERVIOUS	=	84,551 S.F.	28.7%
OPEN SPACE	=	143,291 S.F.	48.6%
TOTAL	=	294,489 S.F.	100.0%

QUANTITIES

SANITARY SEWER
PUBLIC- REFER TO PUBLIC IMPROVEMENT DRAWINGS

532 LF.	8-INCH SANITARY SEWER
2 EA.	8"X8"X8" TEE
2 EA.	8"X8"X8" TEE
	8-INCH VALVE
	8-INCH 11.25" BEND
	8-INCH 22.5" BEND
	8-INCH 45" BEND
	HYDRANT, TEE AND 6-INCH VALVE
	6-INCH SERVICES W/MAINFOLD (SEE DETAIL)
	4-INCH SERVICES W/MAINFOLD (SEE DETAIL)

WATER MAIN
PUBLIC- REFER TO PUBLIC IMPROVEMENT DRAWINGS

1,760 LF.	8-INCH WATER MAIN
4 EA.	8"X8"X8" TEE
7 EA.	8-INCH VALVE
3 EA.	8-INCH 11.25" BEND
5 EA.	8-INCH 22.5" BEND
4 EA.	8-INCH 45" BEND
6 EA.	HYDRANT, TEE AND 6-INCH VALVE
4 EA.	6-INCH SERVICES W/MAINFOLD (SEE DETAIL)
12 EA.	4-INCH SERVICES W/MAINFOLD (SEE DETAIL)

STORM SEWER QUANTITIES
PRIVATE-

56 LF.	12-INCH RCP CL III
1,002 LF.	15-INCH RCP CL III
34 LF.	24-INCH RCP CL III
2 EA.	12-INCH RCP F.E.S.
1 EA.	24-INCH RCP F.E.S.
6 EA.	5W-501 INTAKE W/5W-603 TYPE R CASTING
1 EA.	5W-505 INTAKE W/5W-603 TYPE R CASTING
2 EA.	48" DIA. 5W-401 MANHOLE W/5W-602 TYPE E CASTING
3 EA.	24" DIA. 5W-512 AREA INTAKE W/5W-604 TYPE 4B GRATE

SANITARY SEWER NOTES:

- ALL SANITARY SEWER SERVICES SHALL BE SDR 35 IN ACCORDANCE WITH SUDAS. ALL SERVICES SHALL BE EXTENDED TO WITHIN 5 FOOT OF THE BUILDING FOUNDATION.
- THE UTILITY CONTRACTOR SHALL COORDINATE BUILDING SERVICE CONNECTIONS WITH THE BUILDING MECHANICAL CONTRACTOR.
- ALL 8" SANITARY SEWER SHALL BE PVC TRUSS PIPE WITH CLASS "F-3" BEDDING UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- ALL MANHOLES WITHIN PAVEMENT SHALL HAVE TYPE "B" ADJUSTABLE CASTINGS AND INTERNAL CHIMNEY SEALS. ALL MANHOLES NOT WITHIN PAVEMENT SHALL HAVE TYPE "A" NON-ADJUSTABLE CASTINGS AND EXTERNAL CHIMNEY SEALS.
- MANHOLE STEPS ARE REQUIRED IN ALL SANITARY SEWER MANHOLES.
- SEE SUDAS FOR TYPICAL SANITARY SEWER AND MANHOLE DETAILS.
- CORE DRILL MANHOLE FOR NEW 8" CONNECTION AND FOUR WELL-DEFINED INVERT.
- ALL SEWERS SHALL BE CONSTRUCTED UNDER PLUMBING PERMIT.

STORM SEWER NOTES:

- ALL STORM SEWER IS PRIVATE UNLESS NOTED OTHERWISE.
- ALL INTAKES WITHIN PAVED AREAS SHALL HAVE VANE GRATES.
- 8-INCH STORM SEWER TO BE PVC SDR 35.
- INTAKE CASTING TYPES SHALL FOLLOW THE SUDAS 5W-603 SPECIFICATION. MANHOLE CASTING TYPES SHALL FOLLOW THE SUDAS 5W-602 SPECIFICATION.
- ALL P.E.S.'S SHALL HAVE CONCRETE FOOTINGS PER SUDAS FIGURE 4030.221. THE LAST THREE SECTIONS OF PIPE SHALL BE TIED & APRON GUARDS SHALL BE PROVIDED.

PAVING NOTES:

- ALL ELEVATIONS ARE PROPOSED FINISHED GRADE AT TOP OF SLAB, UNLESS NOTED OTHERWISE.
- COMPACT PAVEMENT SUBGRADE TO 45% STANDARD PROCTOR DENSITY FOR A DEPTH OF 1-FOOT. MOISTURE CONTENT SHALL BE +/- TO +/-4% OF OPTIMUM. SUBGRADE TO BE COMPACTED TO 9-INCH LIFTS.
- ALL EXPOSED CONCRETE SHALL HAVE 6-1% ENTRAINED AIR, F_c = 4,000 P.S.I. MIN. AND ALL AGGREGATE SHALL MEET ASTM C-33.
- PAVING SHALL BE 6-INCH NON-REINFORCED P.C.C. W/6-INCH INTEGRAL CURB & GUTTER. DROP CURB WILL BE PROVIDED AT SIDEWALK RAMPS AND ACCESSIBLE LOCATIONS WHERE NOTED. THE DRIVEWAY APPROACH FROM THE EDGE OF THE STREET THROUGH THE BACKSIDE OF THE SIDEWALK SHALL BE MINIMUM 1-INCH THICKNESS.
- THE DRIVE APPROACH AND ADJACENT SIDEWALK ON ORCHARD HILLS DRIVE SHALL BE LAYED OUT AND INSTALLED PRIOR TO THE EASTERN MOST 20 FEET OF FLAT WORK WITHIN THE SITE.
- ALL HANDICAP STALLS SHALL BE AFFIXED WITH VERTICAL SIGNAGE WITH THE INTERNATIONAL SYMBOL FOR ACCESSIBILITY (WHITE ON BLUE) AND INDICATING THE FINE FOR IMPROPER USE.
- SIDEWALK AND DRIVE APPROACH INSTALLATIONS IN PUBLIC R.O.M. TO BE INSPECTED BY THE CITY OF DES MOINES. MINIMUM 24 HOUR NOTICE IS REQUIRED. A PERMIT IS REQUIRED.

WATER NOTES:

- ALL STATIONING IS BASED ON STREET CENTERLINE MEASUREMENTS.
- PIPE MATERIALS: DUCTILE IRON IN ACCORDANCE WITH STANDARD SPECIFICATIONS OR ANMA C400 CLASS 150 PVC INSTALL NO. 10 THIN STANDARD COPPER TRACER WIRE UNDER PIPE, BRASS TRACER WIRE TO SURFACE AT FIRE HYDRANTS, VALVES, AND NEW DEAD ENDS. CONNECT NEW TRACER TO EXISTING.
- THE CONTRACTOR SHALL PROTECT AND BACKFILL AROUND UNDERGROUND UTILITIES. BACKFILL SHALL BE IN SIX-INCH LAYERS, COMPACTED TO 45% STANDARD PROCTOR DENSITY.
- HYDRANTS SHALL BE SET A MAXIMUM OF 3' FROM THE WATER MAIN.
- HYDRANTS, MANHOLE COVERS, AND VALVE BOXES SHALL BE SET TO CONFORM TO FINISHED PAVEMENT ELEVATIONS.
- HYDRANTS TO BE WATEROUS PACER.
- WATER MAIN TO HAVE 5/2 FEET BURIED TYPICAL EXCEPT AT CRITICAL CROSSINGS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING THE AS-BUILT LOCATION OF ALL WATER SERVICES, OBTAINING PERMITS, AND ARRANGING FOR INSPECTIONS
- ALL SERVICES SHALL BE CONSTRUCTED TO WITHIN 5 FEET OF OUTSIDE OF BUILDING WALL.

GRADING & EROSION CONTROL:

- ALL GRADING & EROSION CONTROL NOTES SHOWN ON GRADING / SWPPP PLANS

ZONING:

EXISTING: ORCHARD VIEW P.U.D.

P.U.D.:

ORCHARD VIEW P.U.D. - PARCEL 3

SETBACKS:

MINIMUM SETBACK FROM PROPERTY BOUNDARIES - 35'
MINIMUM BUILDING SEPARATION - 15'

FLOOD ZONE:

ZONE 'X'
FEMA FIRM FLOOD INSURANCE RATE MAP NUMBER 1818C1027F, REVISED OCTOBER 16, 2014.

ZONE 'X'
FEMA FIRM FLOOD INSURANCE RATE MAP NUMBER 1818C1029F, REVISED OCTOBER 16, 2014.

EASEMENT NOTES:

- ALL STORM SEWER EASEMENTS ARE TO BE 20 FEET WIDE OR TWO TIMES THE DEPTH, WHICHEVER IS GREATER.
- ALL SANITARY SEWER EASEMENTS ARE TO BE 30 FEET WIDE OR TWO TIMES THE DEPTH, WHICHEVER IS GREATER.
- THE USE OF PUBLIC UTILITY EASEMENTS IS SUBORDINATE TO THE CITY'S USE OF ITS DESIGNATED EASEMENT.
- SOME LOTS ACCEPT DRAINAGE FROM ADJACENT PROPERTY. BUILDING ON THESE LOTS MUST TAKE INTO ACCOUNT UPSTREAM DRAINAGE.

GENERAL NOTES

- ALL CONSTRUCTION (PUBLIC & PRIVATE) SHALL BE IN ACCORDANCE WITH THE 2015 EDITION OF SUDAS STANDARD SPECIFICATIONS. CONTRACTOR SHALL ARRANGE FOR TESTING AND INSPECTION AND NOTIFY THE FOLLOWING AT LEAST ONE WEEK PRIOR TO BEGINNING CONSTRUCTION:
 - CITY OF NORNALK PUBLIC WORKS DEPARTMENT.
 - NORNALK LAND CO., L.L.C.
 - CIVIL ENGINEERING CONSULTANTS, INC.
 - IOWA ONE-CALL
- THE LOCATION OF EXISTING FACILITIES AND APPURTENANCES SHOWN ON THIS PLAN ARE BASED ON AVAILABLE INFORMATION WITHOUT UNCOVERING AND MEASURING TO DETERMINE EXACT FACILITIES LOCATIONS. CIVIL ENGINEERING CONSULTANTS, INC. DOES NOT GUARANTEE THE LOCATION OF EXISTING FACILITIES AS SHOWN, OR THAT ALL EXISTING FACILITIES ARE SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT ALL PUBLIC AND PRIVATE UTILITY PROVIDERS SERVING THIS AREA, AND IOWA ONE CALL, TO DETERMINE THE EXTENT AND PRECISE LOCATION OF EXISTING FACILITIES BEFORE CONSTRUCTION BEGINS.
- THE CONTRACTOR SHALL PROTECT EXISTING ON-SITE FACILITIES FROM DAMAGE RESULTING FROM THE CONTRACTOR'S WORK. IF DAMAGE, BREAKAGE, INTERRUPTION OF SERVICE, ETC. OF EXISTING FACILITIES DOES OCCUR THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE UTILITY'S OWNER. DAMAGE TO UTILITIES OR STRUCTURES SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE UTILITY OWNER.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING ANY FARM TILE DAMAGE DURING CONSTRUCTION AND RECORDING LOCATION OF TILE.
- A GEOTECHNICAL REPORT FOR THIS PROJECT CAN BE OBTAINED BY CONTACTING THE OWNER.
- ANY CHANGES TO THE CONSTRUCTION DRAWINGS DURING CONSTRUCTION SHALL BE APPROVED IN WRITING BY THE CITY OF NORNALK PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR IS RESPONSIBLE FOR ANY CHANGES MADE DURING CONSTRUCTION THAT HAVE NOT BEEN APPROVED IN WRITING BY THE CITY OF NORNALK PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF NORNALK PUBLIC WORKS DEPARTMENT 48-HOURS IN ADVANCE OF ANY WORK BEING PERFORMED ON A HOLIDAY OR WEEKEND.
- ALL CONSTRUCTION STAKING SHALL BE PERFORMED BY A LICENSED ENGINEER OR LAND SURVEYOR.
- A CERTIFIED AS-BUILT GRADING PLAN SHALL BE PROVIDED TO THE CITY OF NORNALK PUBLIC WORKS DEPARTMENT PRIOR TO ACCEPTANCE OF PUBLIC IMPROVEMENTS AND APPROVAL OF THE FINAL PLAT.
- ALL WORK SHALL BE CONDUCTED IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THE PLANS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ALL APPLICABLE SAFETY REGULATIONS.
- THE CONTRACTOR SHALL CONDUCT CLEAN-UP, SURFACE RESTORATION, AND SURFACE REPLACEMENT ACTIVITIES AS CONSTRUCTION PROGRESSES. ALL DEBRIS SPILLED ON THE R.O.M. OR ON ADJACENT PROPERTY SHALL BE PICKED UP BY THE CONTRACTOR AT THE END OF EACH DAY.
- IF DISCREPANCY EXISTS BETWEEN THE DETAILED PLANS AND THE QUANTITIES, THE PLANS SHALL GOVERN.
- THE CITY OF NORNALK REQUIRES ALL HANDICAP ACCESS RAMPS AND LANDING PADS TO BE CONSTRUCTED AS PART OF THE PUBLIC IMPROVEMENTS. SIDEWALKS AND TRAILS ARE NOT TO BE CONSTRUCTED AS PART OF THESE IMPROVEMENTS.

LEGAL DESCRIPTION

A PARCEL OF LAND IN OUTLOT '2', NORNALK ORCHARD VIEW PLAT 2, AN OFFICIAL PLAT RECORDED IN BOOK 2015, PAGE 881 AT THE WARREN COUNTY RECORDER'S OFFICE, CITY OF NORNALK, WARREN COUNTY, IOWA THAT IS MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SE CORNER OF SAID OUTLOT '2'; THENCE N89°00'10"W, 38.70 FEET ALONG THE SOUTH LINE OF SAID OUTLOT '2' TO THE SW CORNER OF SECTION 13, TOWNSHIP 77 NORTH, RANGE 25 WEST OF THE 5TH P.M.; THENCE N89°16'56"W, 464.85 FEET ALONG THE SOUTH LINE OF SAID OUTLOT '2' AND SAID SECTION 13 TO A POINT; THENCE N00°48'03"E, 136.41 FEET TO A POINT; THENCE N28°14'22"W, 304.26 FEET TO A POINT; THENCE N64°43'35"E, 41.06 FEET TO A POINT; THENCE N41°43'24"E, 78.41 FEET TO A POINT; THENCE N44°21'42"E, 78.43 FEET TO A POINT; THENCE N43°24'13"E, 71.60 FEET TO A POINT; THENCE N47°12'36"E, 54.13 FEET TO A POINT; THENCE N51°18'57"E, 54.04 FEET TO A POINT; THENCE N50°25'27"E, 90.00 FEET TO A POINT ON THE EAST LINE OF SAID OUTLOT '2'; THENCE S81°54'39"E, 262.41 FEET ALONG SAID EAST LINE TO A POINT OF CURVATURE; THENCE SOUTHERLY ALONG A CURVE TO THE RIGHT HAVING A RADIUS OF 136.80 FEET AND A CHORD BEARING S19°42'23"E, AN ARC LENGTH OF 402.45 FEET ALONG SAID EAST LINE TO A POINT OF TANGENCY; THENCE S00°04'47"W, 42.44 FEET ALONG SAID EAST LINE TO THE POINT OF BEGINNING AND CONTAINING 6.77 ACRES MORE OR LESS.

CONSTRUCTION SCHEDULE

05/15/16 TO 05/30/16 - CLEARING, ROUGH GRADING, SILT FENCE, SEDIMENT BASIN & TEMPORARY SEEDING
06/15/16 TO 06/15/16 - UTILITY CONSTRUCTION
06/15/16 TO 07/15/16 - PAVING
07/15/16 TO 07/31/16 - FINAL GRADING/SEEDING

GENERAL LEGEND

PROPOSED	EXISTING
--- PLAT BOUNDARY	--- LOT LINE
--- SECTION LINE	○ SANITARY/STORM MANHOLE
--- LOT LINE	○ WATER VALVE
--- CENTERLINE	○ FIRE HYDRANT
--- EASEMENT LINE	○ STORM SEWER SINGLE INTAKE
▽ FLARED END SECTION	○ STORM SEWER DOUBLE INTAKE
□ TYPE 5W-501 STORM INTAKE	○ STORM SEWER ROUND INTAKE
○ TYPE 5W-502 STORM INTAKE	○ FLARED END SECTION
○ TYPE 5W-503 STORM INTAKE	○ DECIDUOUS TREE
○ TYPE 5W-504 STORM INTAKE	○ CONIFEROUS TREE
○ TYPE 5W-505 STORM INTAKE	○ SHRUB
○ TYPE 5W-506 STORM INTAKE	○ POWER POLE
○ TYPE 5W-511 STORM INTAKE	○ STREET LIGHT
○ TYPE 5W-512 STORM INTAKE	○ GUY ANCHOR
○ TYPE 5W-513 STORM INTAKE	○ ELECTRIC TRANSFORMER
○ TYPE 5W-401 STORM MANHOLE	○ GAS METER
○ TYPE 5W-402 STORM MANHOLE	○ TELEPHONE RISER
○ TYPE 5W-403 STORM MANHOLE	○ SIGN
○ TYPE 5W-301 SANITARY MANHOLE	○ UNDERGROUND TELEVISION
○ TYPE 5W-302 SANITARY MANHOLE	○ UNDERGROUND ELECTRIC
○ TYPE 5W-304 SANITARY MANHOLE	○ UNDERGROUND GAS
○ STORM/SANITARY CLEANOUT	○ UNDERGROUND FIBER OPTIC
○ WATER VALVE	○ UNDERGROUND TELEPHONE
○ FIRE HYDRANT ASSEMBLY	○ OVERHEAD ELECTRIC
○ BLOW-OFF HYDRANT	○ SANITARY SEWER WITH SIZE
○ DETECTABLE WARNING PANEL	○ STORM SEWER WITH SIZE
	○ WATER MAIN WITH SIZE
	○ EXISTING CONTOUR
	○ TREELINE
	○ BUILDING SETBACK LINE
	○ PUBLIC UTILITY EASEMENT
	○ MINIMUM OPENING ELEVATION

BENCHMARKS

- BRASS PLUG IN HEADWALL OF REINFORCED BOX CULVERT, AT NORTHWEST CORNER OF INTERSECTION OF IOWA HIGHWAY 28 AND ELM AVENUE. ELEVATION-----874.90
- BURY BOLT ON HYDRANT, AT NORTHEAST CORNER OF INTERSECTION OF ASPEN DRIVE AND ELM AVENUE. ELEVATION-----924.44
- CUT 'X' INTERSECTION SYCAMORE DRIVE AND ELM AVENUE. ELEVATION-----934.71

CERTIFICATIONS

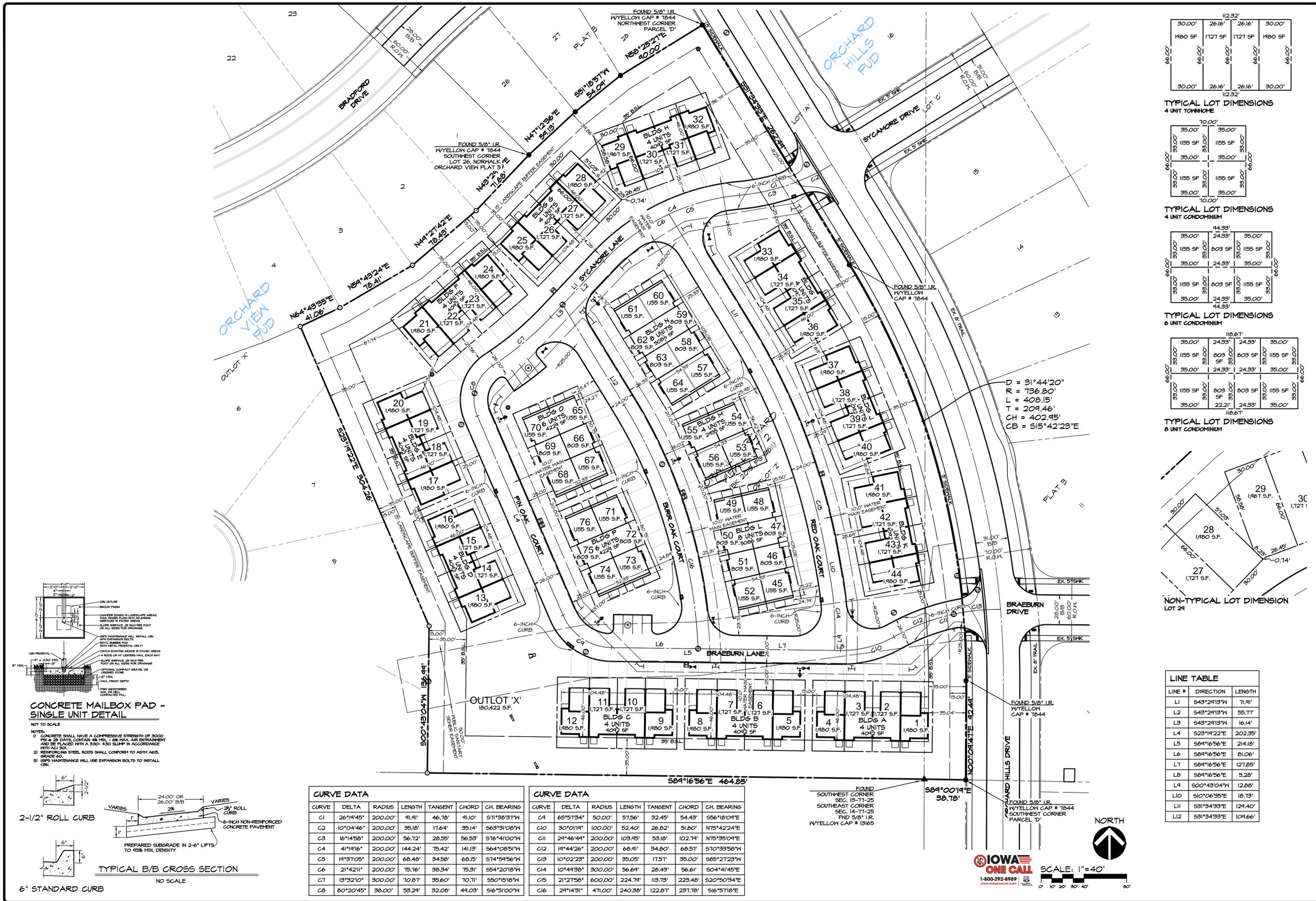
	I HEREBY CERTIFY THAT THE PORTION OF THIS TECHNICAL SUBMISSION DESCRIBED BELOW WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF IOWA. BY: MICHAEL D. MORGAN IOWA REG. NO. 383 DATE: 05/15/2016 PAGES OR SHEETS COVERED BY THIS SEAL: SHEET 5
	I HEREBY CERTIFY THAT THIS LAND SURVEYING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL LAND SURVEYOR UNDER THE LAWS OF THE STATE OF IOWA. JERRY P. OLIVER, IOWA REG. NO. 7844 DATE MY LICENSE RENEWAL DATE IS DECEMBER 31, 2016 PAGES OR SHEETS COVERED BY THIS SEAL:
	I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA. MELISSA M. HILLS, IOWA REG. NO. 16023 DATE MY LICENSE RENEWAL DATE IS DECEMBER 31, 2017 PAGES OR SHEETS COVERED BY THIS SEAL: 1 - 5

Civil Engineering Consultants, Inc.
2400 86th Street, Unit 12, Des Moines, Iowa, 50322
515.276.4884 . Fax: 515.276.7084 . mail@cecinc.com

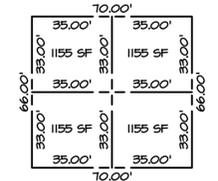


DATE:	REVISIONS	COMMENTS
05/21/2016	1	04/18/2016
	2	
	3	
	4	
	5	
	6	

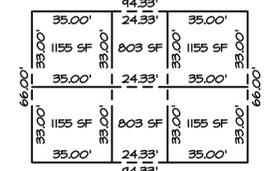
NORNALK ORCHARD VIEW TOWNHOMES
NORNALK, IA
COVER



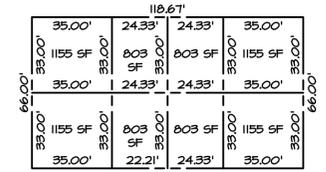
TYPICAL LOT DIMENSIONS
4 UNIT TOWNHOME



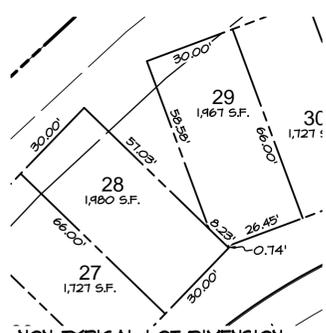
TYPICAL LOT DIMENSIONS
4 UNIT CONDOMINIUM



TYPICAL LOT DIMENSIONS
6 UNIT CONDOMINIUM



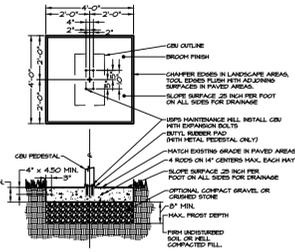
TYPICAL LOT DIMENSIONS
8 UNIT CONDOMINIUM



NON-TYPICAL LOT DIMENSION
LOT 24

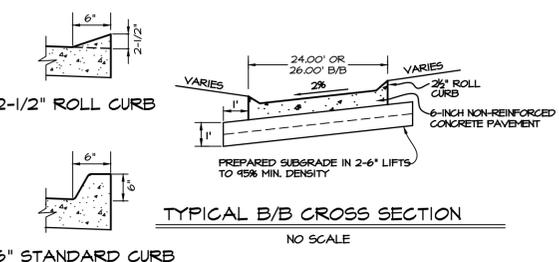
LINE #	DIRECTION	LENGTH
L1	S43°29'13"W	71.91'
L2	S43°29'13"W	55.71'
L3	S43°29'13"W	16.14'
L4	S23°14'22"E	202.35'
L5	S89°16'56"E	214.18'
L6	S89°16'56"E	81.06'
L7	S89°16'56"E	127.85'
L8	S89°16'56"E	5.28'
L9	S00°43'04"W	12.88'
L10	S10°06'35"E	18.73'
L11	S31°34'33"E	124.40'
L12	S31°34'33"E	104.66'

D = 31°44'20"
R = 736.80'
L = 408.15'
T = 209.46'
CH = 402.95'
CB = 515°42'23"E



CONCRETE MAILBOX PAD -
SINGLE UNIT DETAIL
NOT TO SCALE

- NOTES:
- CONCRETE SHALL HAVE A COMPRESSIVE STRENGTH OF 3000 PSI & 28 DAYS, CONTAIN 48 MIN. - 68 MAX. AIR ENTRAINMENT AND BE PLACED WITH A 3/32" - 4/30 SLUMP IN ACCORDANCE WITH ACI 301.
 - REINFORCING STEEL RODS SHALL CONFORM TO ASTM A615, GRADE 60.
 - USPS MAINTENANCE HILL USE EXPANSION BOLTS TO INSTALL CURB.



2-1/2" ROLL CURB
TYPICAL B/B CROSS SECTION
NO SCALE
6" STANDARD CURB

CURVE	DELTA	RADIUS	LENGTH	TANGENT	CHORD	CH. BEARING
C1	26°14'45"	200.00'	91.91'	46.78'	91.10'	S71°38'37"W
C2	10°04'46"	200.00'	35.18'	17.64'	35.14'	S63°31'08"W
C3	16°14'58"	200.00'	56.72'	28.55'	56.53'	S76°41'00"W
C4	41°19'16"	200.00'	144.24'	75.42'	141.13'	S64°08'51"W
C5	19°37'05"	200.00'	68.48'	34.58'	68.15'	S74°54'56"W
C6	21°42'11"	200.00'	75.76'	38.34'	75.31'	S54°20'18"W
C7	13°32'10"	300.00'	70.87'	35.60'	70.71'	S50°15'18"W
C8	80°20'45"	38.00'	53.29'	32.08'	44.03'	S16°51'00"W

CURVE	DELTA	RADIUS	LENGTH	TANGENT	CHORD	CH. BEARING
C9	65°57'34"	50.00'	51.56'	32.45'	54.43'	S56°10'04"E
C10	30°01'14"	100.00'	52.40'	26.82'	51.80'	N75°42'24"E
C11	24°46'44"	200.00'	103.45'	53.18'	102.71'	N75°35'04"E
C12	19°44'26"	200.00'	68.91'	34.80'	68.57'	S70°33'58"W
C13	10°02'23"	200.00'	35.05'	17.57'	35.00'	S85°21'23"W
C14	10°44'38"	300.00'	56.84'	28.43'	56.61'	S04°41'45"E
C15	21°27'58"	600.00'	224.71'	113.73'	223.48'	S20°50'34"E
C16	29°14'31"	471.00'	240.38'	122.87'	237.78'	S16°57'18"E

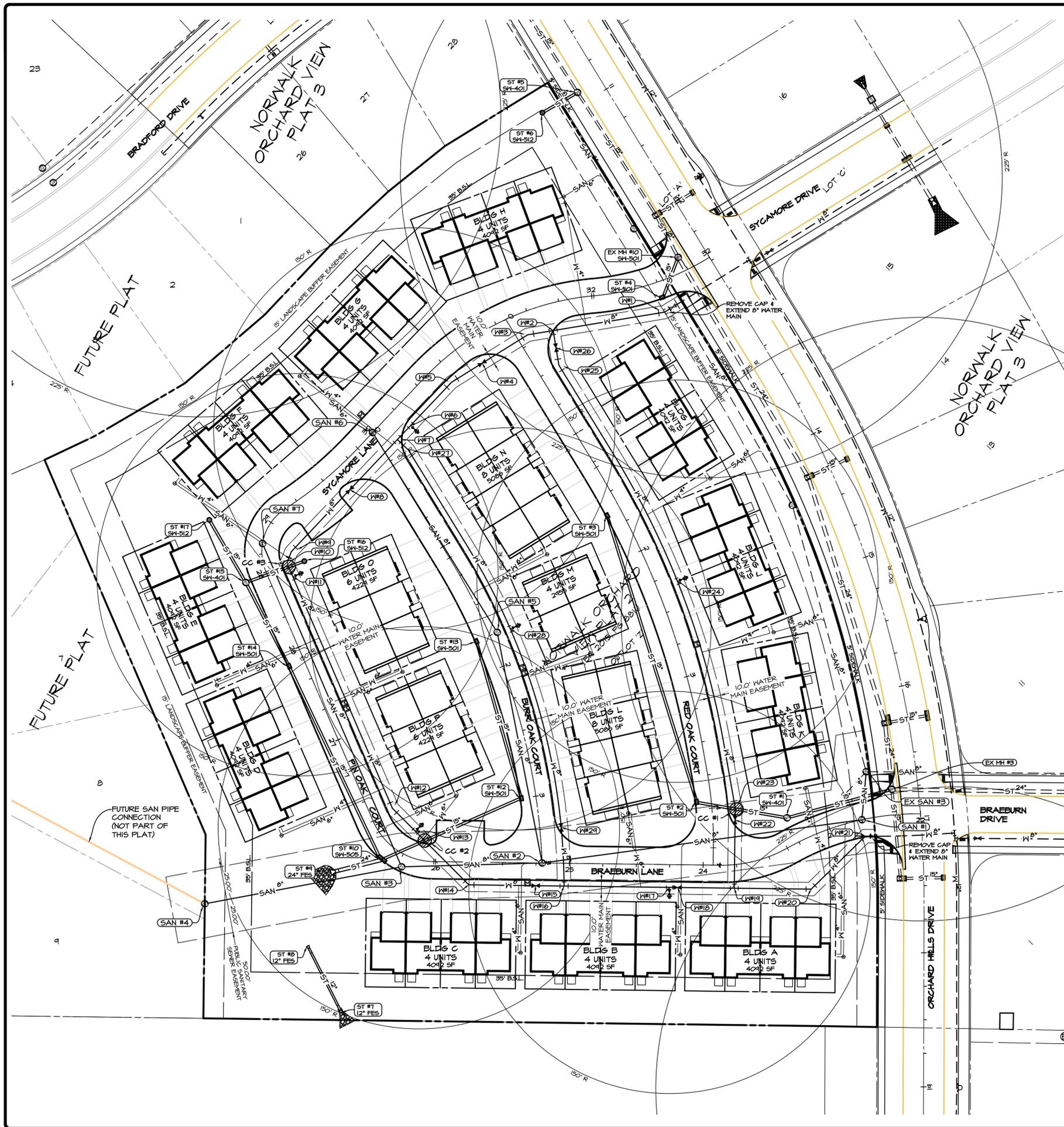
Civil Engineering Consultants, Inc.
2400 86th Street, Unit 12, Des Moines, Iowa, 50322
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DATE	REVISIONS	COMMENTS
09/21/2016	1	04/18/2016
	2	
	3	
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	5	
	6	

NORWALK ORCHARD VIEW TOWNHOMES
NORWALK, IA
DIMENSION PLAN

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SCALE: 1"=40'
0' 10' 20' 30' 40'



NOTE:
 FG ELEVATIONS IN TABLE REPRESENT FINISH GRADE ELEVATION AT STRUCTURES.

WATER STRUCTURE TABLE		
NAME	LOCATION	DETAILS
W#1	STA. 32+40.33, 22.40' RT. &	FG ELEV = 944.06
W#2	STA. 31+65.21, 20.11' RT. &	FG ELEV = 943.91
W#3	STA. 31+44.14, 17.00' RT. &	FG ELEV = 944.34
W#4	STA. 31+00.00, 14.74' RT. &	FG ELEV = 945.14
W#5	STA. 30+70.50, 16.61' RT. &	FG ELEV = 945.85
W#6	STA. 30+25.00, 17.00' RT. &	FG ELEV = 946.36
W#7	STA. 30+10.61, 17.00' RT. &	FG ELEV = 946.21
W#8	STA. 29+60.41, 18.11' RT. &	FG ELEV = 945.95
W#9	STA. 29+16.50, 22.04' RT. &	FG ELEV = 944.30
W#10	STA. 28+61.35, 17.00' RT. &	FG ELEV = 944.54
W#11	STA. 28+50.00, 17.00' RT. &	FG ELEV = 944.37
W#12	STA. 28+26.10, 17.00' RT. &	FG ELEV = 942.17
W#13	STA. 28+25.00, 14.04' RT. &	FG ELEV = 942.20
W#14	STA. 25+75.00, 17.00' LT. &	FG ELEV = 941.80
W#15	STA. 25+25.00, 17.00' LT. &	FG ELEV = 942.30

WATER STRUCTURE TABLE		
NAME	LOCATION	DETAILS
W#16	STA. 25+03.71, 17.02' LT. &	FG ELEV = 942.48
W#17	STA. 24+25.00, 17.00' LT. &	FG ELEV = 943.33
W#18	STA. 24+18.95, 17.00' LT. &	FG ELEV = 943.47
W#19	STA. 23+78.11, 17.44' LT. &	FG ELEV = 943.37
W#20	STA. 23+34.20, 35.44' LT. &	FG ELEV = 943.51
W#21	STA. 22+75.00, 12.46' LT. &	FG ELEV = 940.87
W#22	STA. 4+04.75, 16.70' LT. &	FG ELEV = 942.64
W#23	STA. 4+01.18, 17.42' LT. &	FG ELEV = 942.98
W#24	STA. 2+31.28, 16.00' LT. &	FG ELEV = 943.76
W#25	STA. 0+48.80, 16.00' LT. &	FG ELEV = 944.86
W#26	STA. 0+37.63, 20.97' LT. &	FG ELEV = 944.61
W#27	STA. 0+25.00, 16.00' LT. &	FG ELEV = 946.44
W#28	STA. 1+76.54, 16.00' LT. &	FG ELEV = 944.84
W#29	STA. 3+25.00, 16.00' LT. &	FG ELEV = 943.01

WATER NOTE:
 1. REFER TO PUBLIC IMPROVEMENT DRAWINGS FOR ALL WATER MAIN CONSTRUCTION.

SANITARY STRUCTURE TABLE		
NAME	LOCATION	DETAILS
EX SAN #3	STA. 16+51.90, 37.00' RT. &	RIM = 941.15 IN S = 914.05
48" SW-301 MANHOLE & SW-307 DROP CONNECTION W/SH-301 A CASTING		
SAN #1	STA. 22+75.00, 0.00' &	RIM = 940.61 IN N = 914.29 OUT N = 914.14
48" SW-301 MANHOLE W/SH-601 B CASTING		
SAN #2	STA. 25+20.37, 0.00' &	RIM = 942.06 IN N = 915.36 IN N = 931.00 OUT E = 915.26
48" SW-301 MANHOLE & SW-307 DROP CONNECTION W/SH-601 B CASTING		
SAN #3	STA. 26+22.28, 9.26' LT. &	RIM = 941.06 IN N = 915.88 IN N = 931.00 OUT E = 915.78
48" SW-301 MANHOLE W/SH-601 A CASTING		
SAN #4	STA. 26+57.21, 14.01' LT. &	RIM = 941.96 OUT E = 916.48
48" SW-301 MANHOLE W/SH-601 B CASTING		
SAN #5	STA. 1+75.00, 6.00' LT. &	RIM = 944.47 IN N = 935.10 OUT S = 935.00
48" SW-301 MANHOLE W/SH-601 B CASTING		
SAN #6	STA. 30+01.63, 0.00' &	RIM = 946.44 OUT SE = 937.00
48" SW-301 MANHOLE W/SH-601 B CASTING		
SAN #7	STA. 28+82.98, 0.00' &	RIM = 944.67 OUT SE = 935.67
48" SW-301 MANHOLE W/SH-601 B CASTING		

SANITARY PIPE TABLE			
PIPE	DIA.	LENGTH	SLOPE
EX SAN #3 TO SAN #1	8" PVC	36 LF.	0.40%
SAN #2 TO SAN #5	8" PVC	175 LF.	2.28%
SAN #3 TO SAN #7	8" PVC	263 LF.	1.78%
SAN #1 TO SAN #2	8" PVC	241 LF.	0.40%
SAN #2 TO SAN #3	8" PVC	105 LF.	0.40%
SAN #3 TO SAN #4	8" PVC	150 LF.	0.40%
SAN #5 TO SAN #6	8" PVC	176 LF.	1.08%

STORM PIPE TABLE			
PIPE	DIA.	LENGTH	SLOPE
ST #1 TO EX MH #1	15" RCP CLASS III	81 LF.	4.23%
ST #2 TO ST #1	15" RCP CLASS III	70 LF.	1.22%
ST #3 TO ST #2	15" RCP CLASS III	224 LF.	1.00%
ST #4 TO EX MH #10	15" RCP CLASS III	33 LF.	1.00%
ST #6 TO ST #5	15" RCP CLASS III	30 LF.	1.00%
ST #8 TO ST #7	12" RCP CLASS III	56 LF.	1.00%
ST #10 TO ST #9	24" RCP CLASS III	34 LF.	1.64%
ST #12 TO ST #10	15" RCP CLASS III	113 LF.	2.25%
ST #13 TO ST #12	15" RCP CLASS III	122 LF.	1.43%
ST #14 TO ST #10	15" RCP CLASS III	158 LF.	1.77%
ST #15 TO ST #14	15" RCP CLASS III	70 LF.	1.00%
ST #15 TO ST #16	15" RCP CLASS III	47 LF.	1.00%
ST #17 TO ST #15	15" RCP CLASS III	54 LF.	1.32%

SANITARY NOTE:
 1. REFER TO PUBLIC IMPROVEMENT DRAWINGS FOR CONSTRUCTION OF SANITARY #1 - #4 & EX SAN #3.

SW-401 MANHOLE TABLE		
NAME	LOCATION	DETAILS
EX MH #3	STA. 16+75.86, 14.50' RT. &	RIM = 939.99 IN N = 933.22 OUT E = 931.80
60" SW-401 MANHOLE W/SH-602 F CASTING		
EX MH #10	STA. 12+35.00, 20.00' RT. &	RIM = 942.14 IN S = 938.47 OUT S = 937.47
SW-401 MANHOLE W/SH-602 F CASTING		
ST #3	STA. 29+25.00, 20.00' RT. &	RIM = 942.40 IN N = 936.75 OUT E = 936.65
SW-401 MANHOLE W/SH-602 E CASTING		
ST #5	STA. 10+00.81, 14.62' RT. &	RIM = 944.36 IN S = 940.94 OUT S = 942.84
SW-401 MANHOLE W/SH-602 E CASTING		
ST #15	STA. 28+60.00, 17.00' LT. &	RIM = 944.48 IN E = 934.55 OUT SE = 934.45
SW-401 MANHOLE W/SH-602 E CASTING		

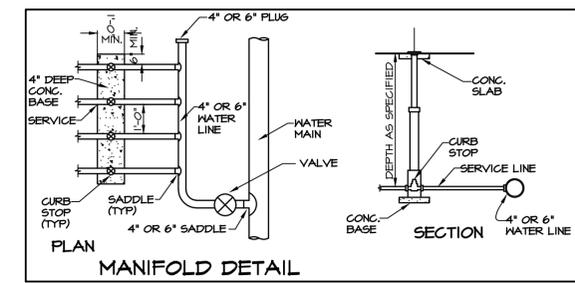
PAVEMENT INTAKE TABLE		
NAME	LOCATION	DETAILS
ST #2	STA. 3+14.22, 12.00' RT. &	GRATE = 942.23 IN N = 937.10 OUT E = 937.60
SW-501 INTAKE W/SH-603 R GRATE		
ST #3	STA. 1+65.00, 12.00' RT. &	GRATE = 943.56 OUT S = 934.93
SW-501 INTAKE W/SH-603 R GRATE		
ST #10	STA. 26+35.10, 13.00' LT. &	GRATE = 941.18 IN NE = 935.84 IN N = 935.84 OUT N = 935.64
SW-505 INTAKE W/SH-603 R GRATES		
ST #14	STA. 27+40.00, 13.00' LT. &	GRATE = 943.14 IN N = 938.75 OUT SE = 938.65
SW-501 INTAKE W/SH-603 R GRATE		

SW-512 INTAKE TABLE		
NAME	LOCATION	DETAILS
ST #6	STA. 10+81.88, 50.00' RT. &	GRATE = 944.84 OUT NE = 941.24
18" SW-512 INTAKE W/SH-604 4A GRATE		
ST #16	STA. 28+57.32, 24.44' RT. &	GRATE = 943.76 OUT N = 940.22
18" SW-512 INTAKE W/SH-604 4A GRATE		
ST #17	STA. 28+88.11, 42.88' LT. &	GRATE = 944.27 OUT SE = 940.26
18" SW-512 INTAKE W/SH-604 4A GRATE		

STORM F.E.S. TABLE		
NAME	LOCATION	DETAILS
ST #7	STA. 26+21.04, 12.60' LT. &	FE = 934.01
12" FES		
ST #8	STA. 26+35.74, 97.47' LT. &	FE = 934.03
12" FES		
ST #1	STA. 26+47.30, 46.86' LT. &	FE = 935.00
24" FES		

STORM NOTE:
 1. EX MH #3 & EX MH #10 MAY NEED TO HAVE RIMS ADJUSTED TO MATCH THE RIM ELEVATIONS SHOWN IN THE TABLE. VERIFY THE ASBUILT ELEVATIONS PRIOR TO CONSTRUCTION.
 2. ST #5 IS TO BE CONSTRUCTED OVER THE EXISTING 15" RCP. VERIFY THE EXISTING RIMS AND POUR CONCRETE COLLARS & WELL DEFINED INVERTS. BASE SHALL BE POURED IN PLACE.

CRITICAL CROSSINGS
 CC#1 FE 15" RCP = 937.21
 F.G. = 942.93
 TOP 8-INCH WATER = 935.50
 CC#2 FE 15" RCP = 936.61
 F.G. = 941.79
 TOP 8-INCH WATER = 934.85
 CC#3 FE 15" RCP = 934.90
 F.G. = 944.49
 TOP 8-INCH WATER = 938.15



NOTE: USE 4" WATER LINE FROM MAIN TO MANIFOLD FOR ALL 4-PLEX BUILDINGS. ALL BUILDINGS GREATER THAN 4 UNITS SHALL HAVE A 6" WATER LINE FROM MAIN TO MANIFOLD.

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DATE:	REVISIONS	COMMENTS
09/21/2016	1	04/18/2016
	2	
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DATE OF SURVEY: 02/15/2016
 DESIGNED BY: MPM
 DRAWN BY: CM

NORWALK ORCHARD VIEW TOWNHOMES
 NORWALK, IA
 UTILITY PLAN

SHEET
 3
 OF 5
 E-7442

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NORTH

SCALE: 1" = 40'



GRADING AND EROSION CONTROL NOTES

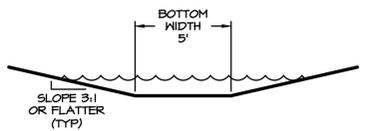
- ALL DIMENSIONS ARE TO BACK OF CURB, OUTSIDE OF BUILDING WALL, AND TO PROPERTY LINES.
- ALL SPOT ELEVATIONS ARE AT GUTTER, UNLESS NOTED OTHERWISE.
- STRIP TOPSOIL FROM ALL AREAS WHICH ARE TO RECEIVE STRUCTURAL FILL.
- AREAS TO RECEIVE FILL TO BE BENCHED.
- PREPARE BOTTOM OF BENCH FOR FILL BY DISCING TO A DEPTH OF 6-INCHES AND COMPACT. ANY LOCALIZED AREAS WHICH CANNOT BE SATISFACTORILY COMPACTED OR WHICH SHOW EVIDENCE OF PUMPING ACTION SHALL BE UNDERCUT AND RECOMPACTED WITH ON-SITE FILL.
- ALL SITE GRADING FILL SHALL BE COMPACTED TO A DENSITY THAT IS NOT LESS THAN 95% STANDARD PROCTOR.
- ALL AREAS WHICH ARE TO RECEIVE PAVING SHALL HAVE THE TOP 12-INCHES DISCED AND RECOMPACTED TO 95% STANDARD PROCTOR DENSITY.
- THE MOISTURE CONTENT OF THE FILL MATERIAL SHALL MATCH URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS, BETWEEN 0 AND 4% OVER OPTIMUM MOISTURE.
- UNLESS GRADING FOR A DESIGNED SUMP OR LOW POINT AREA, GRADE ALL SITE AREAS TO DRAIN WITHOUT TRAPPING OR PONDING SURFACE WATER WHEN SITE GRADING IS COMPLETE.
- FINAL GRADES WITHIN PAVED AREAS SHALL BE WITHIN 0.1' OF PLAN GRADE, ALL OTHER AREAS TO BE WITHIN 0.2' OF PLAN GRADE.
- TOPSOIL SHALL BE RESPREAD TO A MINIMUM THICKNESS OF 4-INCHES ON ALL DISTURBED UNPAVED AREAS.
- BACKFILL TO TOP OF ALL CURBS.
- GRADING & TREE PROTECTION LIMITS SHALL BE STAKED PRIOR TO ANY TREE REMOVAL.
- CIVIL ENGINEERING CONSULTANTS, INC. IS NOT A GEOTECHNICAL ENGINEER.
- A GEOTECHNICAL REPORT FOR THIS PROJECT CAN BE OBTAINED BY CONTACTING THE ENGINEER AND ASKING FOR GEOTECHNICAL EXPLORATION REPORT. THE CONTRACTOR SHALL REFER TO AND FOLLOW THE RECOMMENDATIONS OF ALLENDER-BUTZKE GEOTECHNICAL REPORT PN 191244 DATED OCTOBER 31, 2013.
- STREET PAVEMENT SUBGRADE SHALL BE COMPACTED PER THE GUIDELINES IN THE GEOTECHNICAL EXPLORATION REPORT.
- EROSION CONTROL MEASURES SHALL BE CONSTRUCTED AND INSPECTED IN ACCORDANCE WITH SUDAS SECTION 9040, BY A CERTIFIED PROFESSIONAL IN EROSION & SEDIMENT CONTROL (CPESC). A CPESC IS A RECOGNIZED SPECIALIST IN SOIL EROSION AND SEDIMENT CONTROL, THE SOIL AND WATER CONSERVATION SOCIETY AND THE INTERNATIONAL EROSION CONTROL ASSOCIATION, IN COOPERATION WITH THE AMERICAN SOCIETY OF AGRONOMY, SPONSOR THE CERTIFICATION PROGRAM. CIVIL ENGINEERING CONSULTANTS, INC. IS NOT A CERTIFIED PROFESSIONAL IN EROSION & SEDIMENT CONTROL.
- ALL SLOPES SHALL BE 3:1 OR FLATTER FOR THE SITE.
- A REGIONAL STORM WATER DETENTION & SEDIMENT BASIN IS LOCATED IN ORCHARD TRAIL PLAT 4. REFER TO THE SWPPP PLAN FOR THAT PLAT. NPDES PERMIT NO. 25444-25701. THE REGIONAL BASIN IS LOCATED IN OUTLOT 'Z', ORCHARD VIEW PLAT 4 AND PROVIDES DETENTION FOR THE EAST 3.0 ACRES OF THIS SITE.

MULCHING TABLE

- DRY STRAW OR HAY, 2 TONS PER ACRE. ANCHOR STRAW WITH MULCH TILLER OR ASPHALT TACT @ 1200 PER ACRE.
- WOOD CHIPS OR BARK, 10-12 TONS PER ACRE.

QUANTITIES

143,251 SF	SEEDING (TYPE II EROSION CONTROL MIX)
1,454 LF	SILT FENCE
11 EA	INLET FILTERS
7 TN	DRY STRAW OR HAY MULCH (2 TN / ACRE)
30 TN	CLASS E RIP RAP



GRASS SWALE DETAIL
NO SCALE

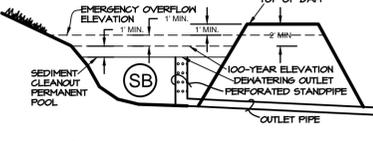
SEEDING NOTES

- FERTILIZER (16-16-16) SHALL BE APPLIED TO THE AREA TO BE SEEDED AT A RATE OF 650 LBS/ACRE.
- THE AREA TO BE SEEDED SHALL BE SMOOTH, AND ALL WASHED AND GULLIES FILLED TO MEET THE DESIRED CROSS SECTION. AREAS ACCESSIBLE TO MACHINERY SHALL BE CULTIVATED TO A DEPTH OF 3". AREAS INACCESSIBLE TO MACHINE SHALL BE CULTIVATED TO A DEPTH OF 1 1/2 INCHES.
- THE FERTILIZER SHALL BE INCORPORATED INTO THE SOIL TO A DEPTH OF 3" WITH A MECHANICAL ROCK PICKER OR A SPRING TOOTH CULTIVATOR.
- ON ALL AREAS ACCESSIBLE TO MACHINERY, A DROP-TYPE SEEDER ATTACHED TO A LANDSCAPE ROLLER SHALL BE USED TO SOW THE GRASS SEED. ON AREAS INACCESSIBLE TO MACHINERY A CYCLONE SEEDER WILL BE PERMITTED. NO OTHER HAND SEEDING METHOD IS ACCEPTABLE.
- ALL SEEDED AREAS SHALL BE MULCHED IMMEDIATELY AFTER SEEDING BY APPLYING 2 TONS OF DRY MULCH PER ACRE.
- THE MULCH MAY CONSIST OF STRAW (OAT, WHEAT, BARLEY OR RYE), HAY, BRONSGRASS, TIMOTHY, ORCHARD GRASS, ALFALFA OR CLOVER SHALL NOT BE USED. ALL MATERIAL MUST BE FREE FROM ALL NOXIOUS WEEDS.
- ALL SEEDED AREAS SHALL BE WATERED ARTIFICIALLY A MINIMUM OF TWICE A DAY FOR THE FIRST WEEK AFTER INSTALLATION, AND ONCE A DAY DURING THE SECOND AND THIRD WEEK AFTER INSTALLATION.

LEGEND

EXISTING/PROPOSED	
	FLAT BOUNDARY
	SANITARY SEWER # SIZE
	STORM SEWER # SIZE
	WATER MAIN # SIZE
	MANHOLE
	STORM INTAKE
	FIRE HYDRANT
	VALVE
	EXISTING FENCE
	SILT FENCE
	INLET FILTER
	EXISTING / PROPOSED STREET TREES
	EXISTING / PROPOSED TREE LINE
	PROPOSED TREE PROTECTION FENCING

SEDIMENT BASIN DETAIL



12" Ø PERFORATED STAND PIPE OVERFLOW + 941.00, 1" MIN. TOP OF STAND PIPE = 931.74, INVERT OUT = 934.00 2 HORIZ. ROWS @ 4.0' O.C., OF 4 - 2" Ø EVENLY SPACED STARTING @ ELEV = 936.40

BENCHMARK
BRASS PLUG IN HEADWALL OF REINFORCED BOX CULVERT AT NORTHWEST CORNER OF INTERSECTION OF IOWA HIGHWAY 20 AND ELM AVENUE.
ELEVATION.....874.90

BURY BOLT ON HYDRANT, AT NORTHEAST CORNER OF INTERSECTION OF ASPEN DRIVE AND ELM AVENUE.
ELEVATION.....424.44

CUT 'X' INTERSECTION SYCAMORE DRIVE AND ELM AVENUE.
ELEVATION.....439.71

GRASS SWALE INSPECTION & MAINTENANCE	
ACTIVITY	SCHEDULE
MOW GRASS TO MAINTAIN HEIGHT OF 3-6 INCHES	AS NEEDED (FREQUENTLY/SEASONALLY)
REMOVE SEDIMENT BUILDUP IN THE BOTTOM OF THE GRASS SWALE ONCE IT HAS ACCUMULATED TO 25% OF ORIGINAL DESIGN VOLUME.	AS NEEDED (INFREQUENTLY)
INSPECT GRASS ALONG SIDE SLOPES FOR EROSION AND FORMATION OF RILLS OR GULLIES AND CORRECT.	ANNUALLY (SEMI ANNUALLY THE FIRST YEAR)
REMOVE TRASH AND DEBRIS ACCUMULATED IN THE CHANNEL.	AS NEEDED BASED ON INSPECTION
BASED ON INSPECTION, PLANT AN ALTERNATIVE GRASS SPECIES IF THE ORIGINAL GRASS COVER HAS NOT BEEN SUCCESSFULLY ESTABLISHED.	ROUTINE

DRY DETENTION/DRY ED BASIN INSPECTION & MAINTENANCE	
ACTIVITY SCHEDULE	
REMOVE DEBRIS FROM BASIN SURFACE TO MINIMIZE OUTLET CLOGGINGS AND IMPROVE AESTHETICS.	ANNUALLY AND FOLLOWING SIGNIFICANT STORM EVENTS
INSPECT INSTALLED LOW-FLOW ORIFICES IN ED BASINS FOR CLOGGING	
REMOVE SEDIMENT BUILDUP	
REPAIR AND RE-VEGETATE ERODED AREAS	
PERFORM STRUCTURAL REPAIRS TO INLET AND OUTLETS	
MOW TO LIMIT UNWANTED VEGETATION	ROUTINE

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NORTH

SCALE: 1" = 40'

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DATE:	REVISIONS	COMMENTS
09/21/2016	1	04/18/2016
	2	
	3	
	4	
	5	PHH
	6	GM

DATE OF SURVEY: 09/19/2016
DESIGNED BY: PHH
DRAWN BY: GM

NORWALK ORCHARD VIEW TOWNHOMES
NORWALK, IA

GRADING PLAN

SHEET **4** OF 5
E-7442



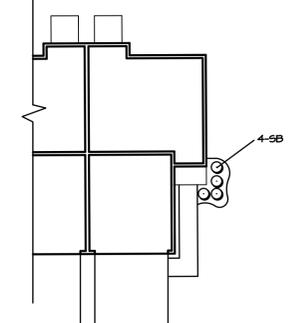
PLANTING SCHEDULE

AR 20	Acer rubrum Red Maple	2" Cal.	B4B	See Plan
QB 4	Quercus bicolor Swamp White Oak	2" Cal.	B4B	See Plan
GT 20	Gleditsia triacanthos 'skyline' Skyline Honeylocust	2" Cal.	B4B	See Plan
UA 4	Ulmus americana American Elm	3" Cal.	B4B	See Plan
PT 7	Populus tremuloides Quaking Aspen	3" Cal.	Cont.	See Plan
CC 14	Crotaegus crusgalli 'cockspur' Thornless Hawthorn	1 1/2" Cal.	Cont.	See Plan
ER 6	Cercis canadensis Eastern Redbud	1 1/2" Cal.	Cont.	Multi-trunk See Plan
MP 21	Malus species Prairiefire Crabapple	1 1/2" Cal.	Cont.	See Plan
PS 6	Pinus strobus White Pine	6'-8" Ht.	TS/B4B	See Plan
PG 20	Picea glauca densata Blackhills Spruce	6'-8" Ht.	TS/B4B	See Plan
AG 11	Abies concolor White Fir	6'-8" Ht.	TS/B4B	See Plan
JC 50	Juniperus chinensis Sea Green Juniper	#3	Cont.	See Plan
CD 85	Corydalis divaricata 'Snow Fairy' Variegated Blue Mist Spirea	#2	Cont.	See Plan
MF 72	Meibomia Florida 'Verweig' Mt. Forest Heigelia	#2	Cont.	See Plan
VT 14	Viburnum trilobum American Cranberry Bush Viburnum	3" Cal.	Cont.	See Plan
DS 46	Dierivilla sessilifolia Cool Spish Honeysuckle	#2	Cont.	See Plan
CA 86	Calamagrostis x acutiflora Karl Foerster Grass	#2	Cont.	See Plan
BW 14	Asclepias tuberosa Butterfly Weed	#2	Cont.	See Plan
SB 20	Spiraea x bumalda 'Anthony Waterer' Anthony Waterer Spirea	#2	Cont.	See Plan
PC 11	Prunus cerasifera Purple Leaf Plum	2" Cal.	Cont.	See Plan

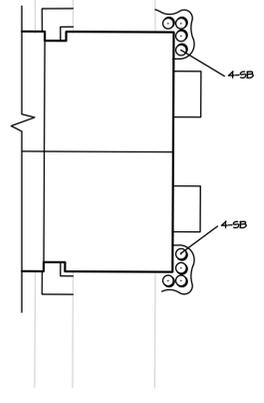
PLANTING NOTES

- ALL SITEMARK, SODDING AND LANDSCAPING SHALL BE IN ACCORDANCE WITH WEST DES MOINES STANDARD SPECIFICATIONS UNLESS SPECIFIED OTHERWISE.
http://ftp.ndm-ia.com/NDM_Metro%20Design%20Standards/
- ALL PLANT MATERIAL SHALL AT LEAST MEET MINIMUM REQUIREMENTS SHOWN IN THE "AMERICAN STANDARD FOR NURSERY STOCK" (ANSI Z60.1-1986).
- CONTRACTOR SHALL GUARANTEE ALL PLANT MATERIALS FOR A PERIOD OF ONE YEAR FROM DATE OF COMPLETION AND ACCEPTANCE BY OWNERS REPRESENTATIVE AFTER INSTALLATION.
- NO PLANT MATERIAL SHALL BE SUBSTITUTED WITHOUT AUTHORIZATION OF LANDSCAPE ARCHITECT AND THE CITY.
- 2" TO 3" CALIPER DECIDUOUS TREES SHALL BE STAKED (2 STAKES) AND WRAPPED IMMEDIATELY AFTER PLANTING. ALL CONIFERS SHALL BE STAKED (2 STAKES).
- ALL TREES, SHRUBS, BEDS & GROUND COVERS SHALL BE MULCHED WITH AT LEAST 3" SHREDDED BARK MULCH.
- PLANT QUANTITIES ARE FOR CONTRACTORS CONVENIENCE, DRAWING SHALL PREVAIL WHERE CONFLICT OCCURS.
- ONE WEEK PRIOR TO INSTALLATION, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT CIVIL ENGINEERING CONSULTANTS, INC. (TELEPHONE 515-276-4884).
- THE CONTRACTOR SHALL STAKE LOCATION OF PLANTS FOR LANDSCAPE ARCHITECT'S APPROVAL BEFORE DIGGING HOLES.
- ALL DECIDUOUS TREES SHALL BE PLANTED AT LEAST 5' FROM R.O.M. AND CONIFEROUS TREES AT LEAST 10' FROM R.O.M.
- ALL DISTURBED AREAS SHALL BE SEEDDED OR SODDED AS SHOWN ON DRAWING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF IDENTIFICATION TAGS, CONTAINERS, AND BURLAP CORDS ON ALL PLANT MATERIAL PRIOR TO COMPLETION OF THE PROJECT.
- ALL METERS AND MECHANICAL TO BE SCREENED BY PLANTINGS OR SCREEN WALL.
- NO OVERSTORY TREES ARE ALLOWED WITHIN PUBLIC SANITARY AND STORM SEWER EASEMENTS.
- SEEDING
 - SOW SEED WITH DRILL-TYPE SEEDING MACHINE. EVENLY DISTRIBUTE SEED BY SOWING EQUAL QUANTITIES IN TWO DIRECTIONS AT RIGHT ANGLES TO EACH OTHER.
 - DO NOT USE WET SEED OR SEED THAT IS MOLDY OR OTHERWISE DAMAGED.
 - DO NOT SEED AGAINST EXISTING TREES. LIMIT EXTENT OF SEED TO OUTSIDE EDGE OF PLANTING SAUCER.
 - SOW SEED AT A TOTAL RATE OF 6 lb/1000 sf.
 - RAKE SEED LIGHTLY INTO TOP 1/8 INCH OF SOIL, ROLL LIGHTLY, AND WATER WITH FINE SPRAY.
 - PROTECT SEEDDED AREAS WITH SLOPES EXCEEDING 1:4 WITH EROSION-CONTROL BLANKETS AND 1:6 WITH EROSION-CONTROL FIBER MESH INSTALLED AND STAPLED ACCORDING TO MANUFACTURER'S WRITTEN INSTRUCTIONS.
 - PROTECT SEEDDED AREAS WITH SLOPES NOT EXCEEDING 1:6 BY SPREADING STRAW MULCH. SPREAD UNIFORMLY AT A MINIMUM RATE OF 2 TONS/ACRE TO FORM A CONTINUOUS BLANKET 1-1/2 INCHES IN LOOSE DEPTH OVER SEEDDED AREAS. SPREAD BY HAND, BLOWER, OR OTHER SUITABLE EQUIPMENT.
 - ANCHOR STRAW MULCH BY CRIMPING INTO SOIL WITH SUITABLE MECHANICAL EQUIPMENT.
 - PROTECT SEEDDED AREAS FROM HOT DR, DRY WEATHER OR DRYING WINDS BY APPLYING COMPOST MULCH UNIFORMLY TO A DEPTH OF 3/16 INCH, AND ROLL SURFACE SMOOTH.
- SODDING
 - LAY SOD WITHIN 24 HOURS OF HARVESTING. DO NOT LAY SOD IF DORMANT OR IF GROUND IS FROZEN OR MUDDY.
 - LAY SOD TO FORM A SOLID MASS WITH TIGHTLY FITTED JOINTS, BUTT ENDS AND SIDES OF SOD; DO NOT STRETCH OR OVERLAP. STAGGER SOD STRIPS OR PADS TO OFFSET JOINTS IN ADJACENT COURSES. AVOID DAMAGE TO SUBGRADE OR SOD DURING INSTALLATION. TAMP AND ROLL LIGHTLY TO ENSURE CONTACT WITH SUBGRADE. ELIMINATE AIR ROCKETS, AND FORM A SMOOTH SURFACE. WORK SIFTED SOIL OR FINE SAND INTO MINOR CRACKS BETWEEN PIECES OF SOD; REMOVE EXCESS TO AVOID SMOTHERING SOD AND ADJACENT GRASS.
 - LAY SOD ACROSS ANGLE OF SLOPES EXCEEDING 1:3.
 - ANCHOR SOD ON SLOPES EXCEEDING 1:6 WITH WOOD PEGS OR STEEL STAPLES SPACED AS RECOMMENDED BY SOD MANUFACTURER BUT NOT LESS THAN 2 ANCHORS PER SOD STRIP TO PREVENT SLIPPAGE.
 - SATURATE SOD WITH FINE WATER SPRAY WITHIN TWO HOURS OF PLANTING. DURING FIRST WEEK AFTER PLANTING, WATER DAILY OR MORE FREQUENTLY AS NECESSARY TO MAINTAIN MOIST SOIL TO A MINIMUM DEPTH OF 1-1/2 INCHES BELOW SOD.
 - ISLANDS TO BE SODDED WITH MUCH RINGS AT TREE LOCATIONS PER DETAIL.
 - THE 30 FOOT BUFFER PARK LANDSCAPING ALONG E. P. TRUE PARKWAY WILL BE INSTALLED BY THE DEVELOPER AS A PART OF THE JORDAN WEST OVERLAY DISTRICT SITE PLAN.

TYPICAL PLANTING DETAIL (SINGLE SIDDED TOWNHOMES)



TYPICAL PLANTING DETAIL (DOUBLE SIDDED TOWNHOMES)



LANDSCAPE REQUIREMENTS

OPEN SPACE
REQUIRED: 2 TREE & 3 SHRUBS/3000 SF
294,484 X 30% = 88,347
88,347 SF/1500 = 59
88,347 SF/1500 = 04
04 SHRUBS

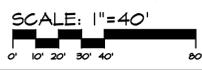
PROPOSED:
54 TREES
04 SHRUBS

15' BUFFER
REQUIRED: 2.0 OVERSTORY, 1 EVERGREEN
TREES, 2.0 UNDERSTORY & 22 SHRUBS/100 LF
1633 LF / 100 = 17
48 OVERSTORY
119 EVERGREEN TREES
48 UNDERSTORY
374 SHRUBS

PROVIDED:
48 OVERSTORY
119 EVERGREEN TREES
48 UNDERSTORY
374 SHRUBS

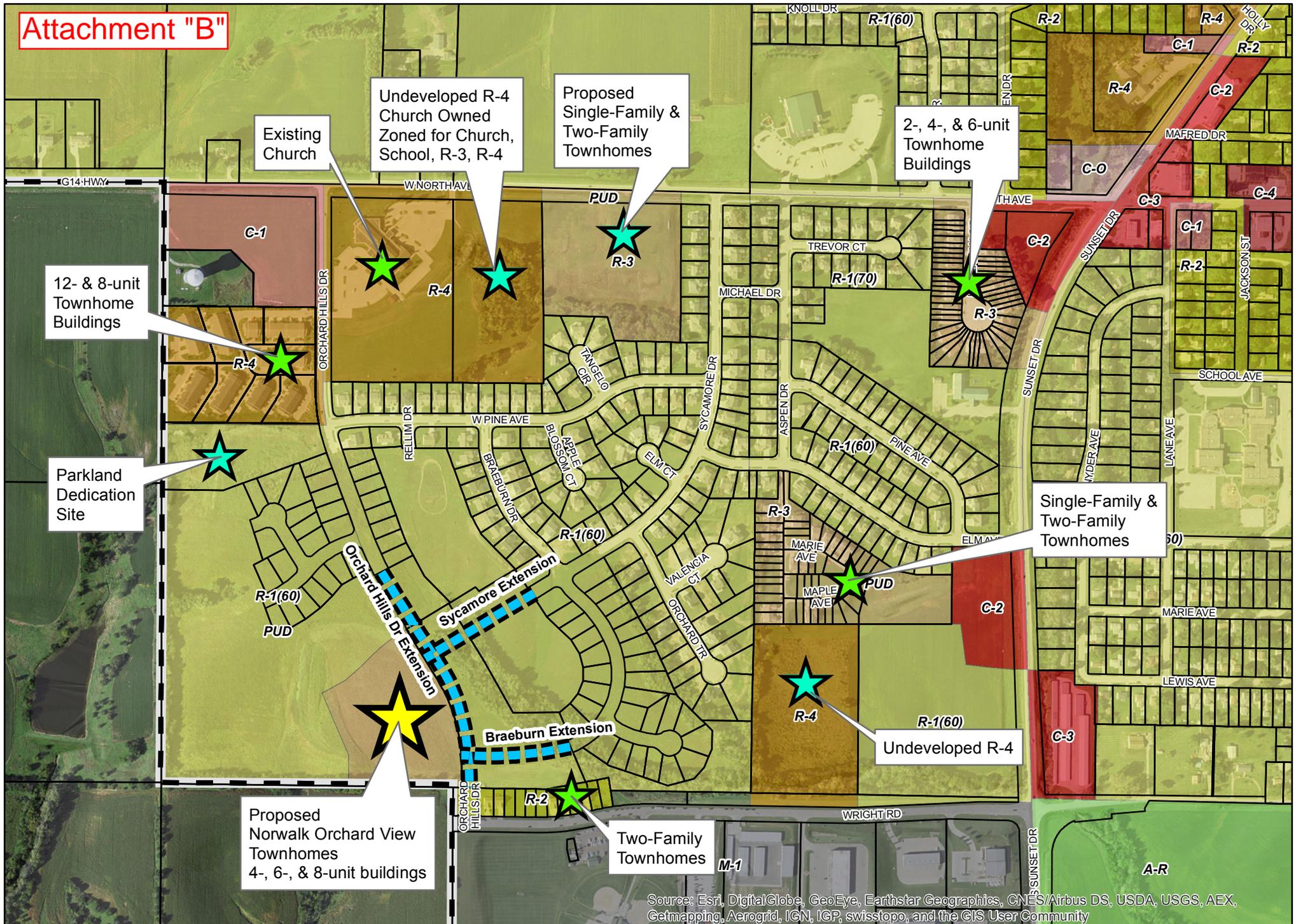


DATE:	REVISIONS	COMMENTS
09/21/2016	1	04/18/2016
	2	
	3	
	4	
	5	
	6	



Q:\E-FILES\2007\742_CED Drawings\Construction Documents\05 LANDSCAPE.dwg, 4/26/2016, 4:23:59 PM, cecinc.com, 1:1

Attachment "B"



Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



Norwalk Orchard View Townhomes Location



Attachment "C"



1 NORTH ELEVATION
1/4" = 1'-0"



2 EAST ELEVATION
1/4" = 1'-0"

EXTERIOR MATERIAL LEGEND

MAT. LABEL	MATERIAL DESCRIPTION MATERIAL SPECIFICATION INFORMATION
SIDING-1A	VINYL LAP SIDING COLOR: LIGHT BLUE
SIDING-1B	VINYL LAP SIDING COLOR: LIGHT GREEN
SIDING-1C	VINYL LAP SIDING COLOR: LIGHT BROWN
SIDING-1D	VINYL LAP SIDING COLOR: LIGHT TAN-GRAY
SIDING-1E	VINYL LAP SIDING COLOR: NATURAL CLAY
SIDING-2A	VINYL, SHAKE STYLE SIDING COLOR: BLUE-GRAY
SIDING-2B	VINYL, SHAKE STYLE SIDING COLOR: DARK GREEN
SIDING-2C	VINYL, SHAKE STYLE SIDING COLOR: BROWN
SIDING-2D	VINYL, SHAKE STYLE SIDING COLOR: DARK TAN-GRAY
STONE	STONE VENEER: BUFF, GROUTED NATURAL MORTAR
TRIM	ALUMINUM WRAPPED WOOD TRIM, WHITE SIZE AS NOTED
WINDOW	VINYL WINDOW, SIZE & TYPE NOTED ON DRAWINGS SUPPLIER TO VERIFY EGRESS DIMENSIONS AT ALL BEDROOMS
ENTRY DOOR	METAL INSULATED DOOR
PATIO DOOR	SLIDING VINYL DOOR
GARAGE DOOR	INSULATED PREFINISHED METAL
DECK	TREATED WOOD SEE PLANS FOR FRAMING NOTES
FASCIA	PREFINISHED METAL FASCIA AND VENTED SOFFIT
ROOF	ASPHALT SHINGLES COLOR: WEATHERED WOOD
SHUTTER	DECORATIVE VINYL SHUTTER COLOR: BLACK
LIGHT	DECORATIVE LIGHT FIXTURE

GENERAL EXT. ELEVATION NOTES

- REFER TO ADDITIONAL NOTES AND REQUIREMENTS ON ALL OTHER DOCUMENTS AND OTHER DISCIPLINES DOCUMENTS.
- ALL HORIZONTAL TRANSITIONS BETWEEN DIFFERENT MATERIALS SHALL BE FLASHED CONTINUOUS FROM UP BEHIND TO UNDER UPPER MATERIAL AND EXTEND OVER TO EXTERIOR FACE OF LOWER MATERIAL.
- ANY RETURNS OR BLIND ELEVATIONS NOT SHOWN SHALL BE SIMILAR IN MATERIAL AND MAKEUP TO ADJACENT CONDITIONS OR OTHER SIMILAR CONDITIONS.
- INSTALL VERTICAL EXPANSION JOINT MINIMAL AT THE HEAD OF DOOR JAMBS/OPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT AT MAIN STRUCTURAL BEARING, KEEP AWAY 2"-6" MIN.
- CAULK & SEAL ALL TRANSITION-CONTROL-EXPANSION AT ALL EXTERIOR MATERIALS.
- INSULATE AND SEAL TO ENSURE ADEQUATE AIR INFILTRATION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER PIPE FREEZING.
- ADDRESS SHALL BE PROVIDED ON STREET SIDE OF BUILDING. NUMBERS A MINIMUM 5 INCH CONTRASTING COLOR NUMBERS WITH MINIMUM 1/2" STROKE.
- SEE SPECIFICATION NOTES, SHEET A0.1 FOR FLASHING NOTES

NORWALK ORCHARD
VIEW TOWNHOMES
NORWALK, IOWA
04-26-2016

SAA# 16046

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1 SOUTH ELEVATION
1/4" = 1'-0"



2 WEST ELEVATION
1/4" = 1'-0"

EXTERIOR MATERIAL LEGEND

MAT. LABEL	MATERIAL DESCRIPTION MATERIAL SPECIFICATION INFORMATION
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SIDING-1B	VINYL LAP SIDING COLOR: LIGHT GREEN
SIDING-1C	VINYL LAP SIDING COLOR: LIGHT BROWN
SIDING-1D	VINYL LAP SIDING COLOR: LIGHT TAN-GRAY
SIDING-1E	VINYL LAP SIDING COLOR: NATURAL CLAY
SIDING-2A	VINYL, SHAKE STYLE SIDING COLOR: BLUE-GRAY
SIDING-2B	VINYL, SHAKE STYLE SIDING COLOR: DARK GREEN
SIDING-2C	VINYL, SHAKE STYLE SIDING COLOR: BROWN
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ENTRY DOOR	METAL INSULATED DOOR
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GARAGE DOOR	INSULATED PREFINISHED METAL
DECK	TREATED WOOD SEE PLANS FOR FRAMING NOTES
FASCIA	PREFINISHED METAL FASCIA AND VENTED SOFFIT
ROOF	ASPHALT SHINGLES COLOR: WEATHERED WOOD
SHUTTER	DECORATIVE VINYL SHUTTER COLOR: BLACK
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- ANY RETURNS OR BLIND ELEVATIONS NOT SHOWN SHALL BE SIMILAR IN MATERIAL AND MAKEUP TO ADJACENT CONDITIONS OR OTHER SIMILAR CONDITIONS.
- INSTALL VERTICAL EXPANSION JOINT MINIMAL AT THE HEAD OF DOOR JAMBS/OPENINGS OR AS OTHERWISE NOTED OR WHERE REQUIRED PER STANDARD MATERIAL PRACTICE TO REDUCE STRESS CRACKING. NO CONTROL JOINT/EXPANSION JOINT AT MAIN STRUCTURAL BEARING, KEEP AWAY 2"-6" MIN.
- CAULK & SEAL ALL TRANSITION-CONTROL-EXPANSION AT ALL EXTERIOR MATERIALS.
- INSULATE AND SEAL TO ENSURE ADEQUATE AIR INFILTRATION BARRIER AROUND ALL EXTERIOR PENETRATIONS TO PREVENT WATER PIPE FREEZING.
- ADDRESS SHALL BE PROVIDED ON STREET SIDE OF BUILDING. NUMBERS A MINIMUM 5 INCH CONTRASTING COLOR NUMBERS WITH MINIMUM 1/2" STROKE.
- SEE SPECIFICATION NOTES, SHEET A0.1 FOR FLASHING NOTES

**NORWALK ORCHARD
VIEW TOWNHOMES**
NORWALK, IOWA
04-26-2016

SAA# 16046

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phn 515 440 5626 www.simonsonassoc.com



REAR ELEVATION

SCALE: 1/8"=1'-0"



RIGHT ELEVATION

SCALE: 1/8"=1'-0"



LEFT ELEVATION

SCALE: 1/8"=1'-0"



FRONT ELEVATION

SCALE: 1/8"=1'-0"

TYPICAL NOTES:

2009 INTERNATIONAL BUILDING CODE:

1. TYPICAL CONSTRUCTION ASSEMBLIES
 - A. ROOF CONSTRUCTION:
 - COMPOSITE ROOF SHINGLES ON #30 FELT ON 1/2" OSB ROOF SHEATHING.
 - 3/4" FIRE RESISTIVE PLYWOOD ROOF SHEATHING REQUIRED ON ROOF FRAMING MEMBERS AS NOTED ON ROOF PLANS.
 - CEILING:
 - FRAMED 16" O.C. = 1/2" GYPSUM BOARD
 - FRAMED 24" O.C. = 5/8" GYPSUM BOARD OR 1/2" CEILING GYPSUM BOARD
 - B. EXTERIOR WALL CONSTRUCTION:
 - SIDING (AS NOTED ON ELEVATIONS) ON HOUSE WRAP OVER EXTERIOR WALL SHEATHING AS NOTED BELOW:
 - 7/16" OSB ON FRONT ELEVATION, U.N.O.
 - RIGID FOAM SHEATHING ON SIDE AND REAR WALLS
 - 7/16" OSB ON FACE OF ALL GABLES
 - 7/16" OSB ON ALL INSIDE AND OUTSIDE CORNERS
 - INTERIOR: 1/2" GYPSUM WALL BOARD OVER 4 MIL POLY. VAPOR BARRIER.
 - C. EXTERIOR MASONRY:
 - STONE VENEER OVER HOUSE WRAP ON 7/16" OSB BRICK VENEER W/ MASONRY TIES 16" O.C. HORIZONTAL & VERTICAL OVER HOUSE WRAP ON 7/16" OSB
 - D. COMMON WALL CONSTRUCTION:
 - 2- HOUR FIRE RESISTIVE ASSEMBLY:
 - 2- PARALLEL 1 HOUR RATED 2X4 WALLS FRAMED 16" O.C. AND SPACED 3' APART (FRAME DIMENSION) WITH 1 LAYER OF 5/8" TYPE "X" GYPSUM WALLBOARD ON EACH FACE OF EACH WALL. MAINTAIN SEPARATION FROM TOP OF FOUNDATION TO UNDERSIDE OF ROOF DECK. (SEE DETAIL SHEET D1)
 - COMMON GARAGE WALL CONSTRUCTION:
 - 2- HOUR FIRE RESISTIVE ASSEMBLY:
 - 2- PARALLEL 1 HOUR RATED 2X4 WALLS FRAMED 16" O.C. AND SPACED 3' APART (FRAME DIMENSION) WITH 1 LAYER OF 5/8" TYPE "X" GYPSUM WALLBOARD ON EACH FACE OF EACH WALL. MAINTAIN SEPARATION FROM TOP OF FOUNDATION TO UNDERSIDE OF ROOF DECK. (SEE DETAIL SHEET D1)
 - E. BASEMENT AND GARAGE FLOOR CONSTRUCTION:
 - 4" CONCRETE SLAB (MINIMUM) ON COMPACTED GRANULAR FILL.
 - F. FRAME FLOOR CONSTRUCTION:
 - FINISH FLOORING (AS NOTED ON FLOOR PLANS) ON 3/4" FLOOR SHEATHING ON ENGINEERED FLOOR TRUSSES OR AS NOTED (SIZE AND SPACING AS NOTED ON FLOOR PLANS).
 - MAIN FLOOR CEILING: 5/8" GYPSUM BOARD OR 1/2" CEILING GYPSUM BOARD
 - FRAMED 24" O.C. = 5/8" GYPSUM BOARD OR 1/2" CEILING GYPSUM BOARD
 - FRAMED 16" O.C. = 1/2" GYPSUM BOARD
2. WINDOWS:
 - ROUGH OPENING OF WINDOWS TO BE NOTED IN FT./IN. ON FLOOR PLANS, U.N.O.
 - TYPICAL HEAD HEIGHTS FOR WINDOWS TO BE 6'-11 1/2", U.N.O.
 - A. CASSEMENT WINDOWS
 - B. SLIDING WINDOWS
 - C. NOTED ON PLANS AS SL
 - D. NOTED ON PLANS AS SH
 - E. DOUBLE HUNG WINDOWS
 - F. NOTED ON PLANS AS DH
3. DOORS:
 - DOOR SIZES NOTED ON FLOOR PLANS IN FT./IN. ON FLOOR PLANS, U.N.O.
 - TYPICAL HEAD HEIGHT FOR DOORS TO BE 6'-11"
 - A. MIXED LAMBS AND CASING
 - B. ROUGH OPENING FOR HINGED DOORS TO BE 2" WIDER THAN DOOR SIZES NOTED ON PLAN.
 - C. BI-FOLD DOORS TO BE 2-1/4" WIDER THAN DOOR SIZE NOTED ON PLANS.
 - D. ROUGH OPENING FOR BI-PASS DOORS TO BE 1" WIDER THAN DOOR SIZE NOTED ON PLANS.
 - E. GYPSUM BOARD OPENINGS
 - F. ROUGH OPENINGS FOR BI-PASS DOORS TO BE SAME AS DOOR SIZE NOTED ON PLANS.
 - G. ROUGH OPENINGS FOR BI-FOLD DOORS TO BE 1-1/4" WIDER THAN DOOR SIZE NOTED ON PLAN.

ALL EXTERIOR FRAME DIMENSIONS INCLUDE 1/2" THICK WALL SHEATHING

ADJUST PLACEMENT OF FRAMING MEMBERS AS REQUIRED TO PROVIDE REQUIRED CLEARANCE FOR PLUMBING AND MECHANICAL SYSTEMS

HOLD ALL DOOR AND WINDOW ROUGH OPENINGS 3" (MIN.) FROM INTERSECTING WALLS TO ALLOW FOR TRIM

ALL STRUCTURE AND BEAMS TO BE SIZED BY TRUSS OR JOIST MANUFACTURER

Review Set - Not For Construction - 04.27.16

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Orchard View Townhomes

Address - Norwalk, Iowa
4 Plex Unit - 3 Bedroom- Slab on Grade

Builder:	Happe Homes
Project No.:	[515] 963.0842
Date:	--
Drawn By:	SH/JA/TK
Revisions:	--
Sheet Title:	EXTERIOR ELEVATIONS
Sheet No.:	4-3.1



BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT

Item No. 12
For Meeting of 05.05.16

- REQUEST:** Consideration of second and possible third reading of an ordinance amending the master plan and rules, regulations and guidelines for the Echo Valley Community Planned Unit Development as contained in Ordinance No 03-08.
- STAFF CONTACT:** Luke Parris, AICP
City Planner
- APPLICANT(S):** United Properties LC
- LOCATION:** Northeast of the intersection of Iowa Highway 28 and Beardsley Street
- CURRENT USE:** Echo Valley Community PUD Parcel J with a mix of C-O, C-1, PC, and R-4.
- PROPOSED USE:** Add the lots along Iowa Highway 28 into Parcel J of the PUD and change the uses to a mix of C-3, R-2, R-3, and R-4. Restrict the R-4 uses to senior housing and assisted living.
- ZONING HISTORY:** The site is zoned as Parcel J of the Echo Valley Community PUD in 2003 (Ordinance 03-08). At that time Parcel J did not include the lots along Iowa Highway 28. Those lots along Iowa Highway 28 are currently zoned as C-O.
- LAND USE PLAN:** The future land use plan the majority of this area as General Commercial with a portion shown as Park/Recreation near the Golf Course.
- SURROUNDING LAND USE PLAN AND ZONING:** Surrounding land use planned for the area is:
- North – Medium Density Residential.
 - East – Park/Recreation & Medium Density Residential.
 - South – Sub Area 1.
 - West – High Density Residential.
- Surrounding zoning for the area is:
- North – R-1 Residential.
 - East – R-1 Residential.
 - South – C-O, C-1, C-2 commercial.
 - West – R-1 Residential.

FLOOD INFORMATION:

None.

**MAJOR STREET
PLAN/TRAFFIC:**

The request is in conjunction with a recently approved development agreement with United Properties LC for the removal of Masteller Road and the construction of the new Marketplace Drive. The new Marketplace Drive will have access onto Beardsley Street at a point east of the current access to Masteller Road. The current intersection with Iowa Highway 28 will be maintained and upgraded with a traffic signal.

The street is designated as a 28' local street to promote a more walkable scale in the development. An 8' trail will be located on the east side of Marketplace Drive and future pedestrian considerations will be made as sites develop.

The City currently uses the Statewide Urban Design and Specifications (SUDAS) for details on various City infrastructure. SUDAS would require a 31' local street in a commercial area and a 26' local street in a residential area. The PUD process allows for the deviation of road width standards. The request for a 28' street is less than the SUDAS standard for commercial but more than the SUDAS standard for residential. This site is a mixed use site that will contain both commercial and residential uses. Additionally, the 28' with matches the City's Subdivision Ordinance for street design standards of a general local street.

**DEVELOPMENT SECTOR
ANALYSIS:**

In the development agreement with United Properties LC, the City anticipates that a significant portion of Parcel J will develop commercially in order to provide TIF revenue to pay for Marketplace Drive, the traffic signal, acquisition of the James Oil site, and the expansion of Beardsley Street.

STAFF ANALYSIS:

The future land use plan for the area is identified as General Commercial. The C-3 component of the rezoning request matches with the future land use plan.

The request also contains residential components. United Properties LC has indicated that the residential components are to support the commercial, provide a transition between the existing developments to the east, and provide a walkable style of development in connecting with the existing development. The residential component is not consistent with the future land use plan but does have consistency with the Echo Valley Community PUD that was approved in 2003, prior to the adoption of the 2013 Comprehensive Plan.

It is staff opinion that allowing the residential component will maintain the intent of the original Echo Valley Community PUD while providing an appropriate land use transition between the neighboring single family homes and the proposed commercial sites.

The amendment proposal also includes several changes to the land use densities and bulk regulations that would be standard in the Zoning Ordinance for each district. The full tables can be found on

the attached PUD amendment document.

For land use densities, United Properties is proposing the R-2 be allowed 6 dwelling units per acre, up from the 5 allowed in the Zoning Ordinance. They are also proposing an increase in the density of any R-4 area to 20 dwelling units per acre, up from the 18 allowed in the Zoning Ordinance.

For bulk regulations, the rear setback for C-3 is proposed to be 10'. For double frontage lots along Iowa Highway 28, this would be measured from the lot line along Iowa Highway 28. The side setback is proposed to be 20' for R-4. The maximum height for the R-4 is proposed to be 45' but with a limit of 3 stories for any building. The R-3 is proposed to allow postage stamp lots with a minimum size of 1,250 square feet. The building separation in the R-3 is proposed to be 12' for sides and 20' for the rear of structures. The minimum lot width in the R-2 district is proposed as 40' for one side of a two-family dwelling unit. Side setbacks for one- and two-family units are proposed to be 5' on one side and a total of 10'.

The request for increased density and relaxed setbacks are related to the creation of a more walkable development. Allowing for uses to be closer together and increasing the density, both residentially and commercially, is a key component of creating walkable design.

The PUD also includes additional information that alters the standard requirements of the Zoning Ordinance. First, the R-4 in the PUD is restricted to only senior living and assisted living type facilities. The PUD amendment also addresses buffering of the uses on the Parcel. The amendment requires a buffer wall be built along the northern property boundary to buffer the existing residential from any commercial development. Buffers between uses internal to the site do not require a minimum distance and will be achieved via landscaping.

The buffering on the site is to provide separation from existing uses while allowing the mix of uses on the site to be developed cohesively into one mixed use development.

**STAFF
RECOMMENDATION:**

Staff recommended that the Planning and Zoning Commission approve of the amendment to Parcel J of the Echo Valley Community PUD.

**PLANNING AND ZONING
RECOMMENDATION:**

The Planning and Zoning Commission recommends approval of the proposed amendment with the condition that principal permitted uses in the C-3 district be limited to:

- Any principal permitted use in the C-2 zoning district, provided such use shall comply with the minimum requirements of the C-3 Zoning District;
- Car Washes, including self-service;
- Gas stations or Service Stations
- Lumber yards, retail only

ATTACHMENTS:

Attachment "A" – Echo Valley Community PUD Parcel J Rezoning Map
Attachment "B" – List of allowable uses in the C-2 and C-3 districts

Resolution Ordinance Contract Other (Specify) _____

Funding Source: _____ NA _____



APPROVED FOR SUBMITTAL:

Planning & Economic Development Director

ORDINANCE NO. _____

AN ORDINANCE AMENDING THE MASTER PLAN AND RULES, REGULATION, AND GUIDELINES FOR THE ECHO VALLEY COMMUNITY PLANNED UNIT DEVELOPMENT AS CONTAINED IN ORDINANCE NO. 03-08

BE IT ENACTED BY THE CITY COUNCIL OF THE CITY OF NORWALK, IOWA:

SECTION 1. PURPOSE. The purpose of this ordinance is to amend the master plan and rules, regulation, and guidelines for Echo Valley Community Planned Unit Development as contained in Ordinance No. 03-08.

SECTION 2. AMENDMENT. The Echo Valley Community Planned Unit Development is hereby amended with the following:

Echo Valley Community Planned Unit Development Master Plan Map:

Add the property along Iowa Highway 28 owned by United Properties LC as depicted in the map in Attachment "A"

Section 6: Land Use Design Criteria:

Delete Parcel J from the land use and density schedule and amend with the new Parcel J, as shown below:

Parcel #	Land Use/ Zoning	Density	Area/Acres	# Units	Density DU/Acre
Parcel J	Mix of C-3, R-4, R-3 & R-2	R-2 6 DU/Ac	27.85	N/A	N/A
		R-3 12 DU/Ac			
		R-4 20 DU/Ac			

Delete Parcel J from the bulk regulations and amend with the new Parcel J, as shown below:

	Lot Area	Lot Width	Setbacks				Height Feet
			Front Feet	Side Each Feet	Side Total Feet	Rear Feet	
Parcel J	C-3 – 20,000 SF	100'	30'	10'	20'	10'	50'
	R-4 – 80,000 SF & 1,250/unit	200' Project	35'	20' Project*	N/A	35'	45'***
	R-3 – 3,125/unit or Postage Stamp Lots 1,250/unit	200' project and 20' individual unit	30' Project or 25' to Curb (for private streets)***	0' shared wall and 12' building separation	N/A	30' Project and 20' for lots internal to the project	35'
	R-2 - Two- Family = 12,500	40' with a 0' side yard or 80' for two units on one lot	25'****	5' or 0' shared wall	10'	30'	35'
	R-2 - One- Family = 8,125	65'	25'****	5'	10'	30'	35'

*20' minimum separation between buildings in a complex

**Principal structure may be 45' in height but not exceed 3 floors above grade

***Covered front porches that are fully open and not enclosed may encroach eight (8) feet into the front yard setback.

Under “Specific Information Not In Tables” remove Parcel J and replace with the following:

PARCEL J. This is considered a mixed use parcel that will be primarily commercial in nature. Lots located along Iowa Highway 28 shall conform to the standards of the C-3 Highway Service Commercial District. The rest of the parcel shall be allowed to be a mix of C-3, R-2, R-3, and R-4. No R-2 or R-3 residential units shall be allowed to front on to the new Marketplace Drive. C-3 uses shall be limited to:

- Any principal permitted use in the C-2 zoning district, provided such use shall comply with the minimum requirements of the C-3 Zoning District;
- Car Washes, including self-service;
- Gas stations or Service Stations
- Lumber yards, retail only

For any R-3 uses, postage stamp lots shall be allowed. Postage stamp lots refer to lots that only encompass the dwelling unit of a multi-unit townhome structure with any open space owned by a common home owner’s association entity. R-4 uses shall be limited to:

- Assisted Living Residential Facilities, Boarding House, Nursing or Convalescent Home, Dormitories, or other group quarters, not exceeding eighteen (18) dwelling units per acre of lot area exclusive of public street right-of-way, or for those facilities which do not provide separate living quarters defined as dwelling units within the zoning ordinance, a maximum of thirty-six (36) beds or residents per acre of lot area exclusive of public right-of-way.

Buffering

Any commercial development along the northern boundary of the parcel shall have a buffer wall installed that includes masonry columns with framed wood slats similar to the images included below. Buffers between uses internal to the parcel will be determined as development proceeds. Internal buffers may be achieved via simple landscaping with no minimum buffer width requirement.



Examples on acceptable buffer wall

Road Widths

To promote a more walkable scale, Marketplace Drive is planned to be a 28’ wide street through the majority of the corridor with it widening to a 37’ wide street at the intersection with Beardsley Street. Pedestrian movements will be accommodated by an 8’ wide trail along the east side of Marketplace Drive. Further pedestrian considerations will be made as each site develops.

SECTION 3. SEVERABILITY CLAUSE. In any section, provision, or part of this ordinance shall be adjudged invalid or unconstitutional such adjudication shall not affect the validity of the ordinance as a whole or any section, provision or part thereof not adjudged invalid or unconstitutional.

SECTION 4. EFFECTIVE DATE. This Ordinance shall be in full force and effect after its passage, approval and publication as provided by law.

Passed and approved by the City Council of the City of Norwalk, Iowa on the _____ day of _____, 2016.

Tom Phillips, Mayor

ATTEST:

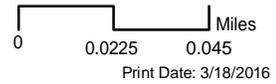
Jodi Eddleman, Deputy City Clerk

PREPARED BY: Luke Parris, City Planner

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Isley	_____	_____
Kuhl	_____	_____
Lester	_____	_____
Livingston	_____	_____
Riva	_____	_____



UNITED PROPERTIES LC REZONE REQUEST MAP



ORDINANCE NO. ~~3~~3-D

AN ORDINANCE TO AMEND THE OFFICIAL ZONING MAP TO REZONE CERTAIN PROPERTY FROM R-1(60), R-1(80), AND R-3 TO PUD AND TO ADOPT THE MASTER PLAN AND ESTABLISH THE RULES, REGULATIONS AND GUIDELINES FOR THE DEVELOPMENT OF THE ECHO VALLEY COMMUNITY PLANNED UNIT DEVELOPMENT.

BE IT ENACTED BY THE CITY COUNCIL OF THE CITY OF NORWALK, IOWA:

- SECTION 1. PURPOSE. The purpose of this ordinance is to change the Official Zoning Map of the City of Norwalk, Iowa, under the provisions of Title 17 (Zoning Regulations), Chapter 17.04 (General Provisions), Section 17.04.060 (Zoning District Boundaries and Official Zoning Map) of the Norwalk Municipal Code, and to adopt a master plan pursuant to Chapter 17.10 (Zoning District Regulations), Section 17.10.170 (PUD, Planned Unit Development), Subsection 17.10.170.3 (Master Plan).
- SECTION 2. OFFICIAL ZONING MAP AMENDED. The official zoning map is amended from R-1(70) and R-1(80), Single Family Residential District, and R-3 Medium Density Residential District, to PUD, Planned Unit Development District. The following legally described property is hereby rezoned from, R-3, R-1(60), and R-1(80) to PUD:

Legal Description

"Exhibit A"

- SECTION 3. PROJECT DESCRIPTION. See Narrative "Exhibit B"
- SECTION 4. PUD AND MASTER PLAN ADOPTION. Attached hereto and made a part of this ordinance for delineation is the Master Plan document for "ECHO VALLEY COMMUNITY" Planned Unit Development, marked Exhibit "C". The Master Plan is adopted to establish rules, regulations, and development guidelines for land use, and performance standards pursuant to Subsection 17.10.170.4 of the Municipal Code of the City of Norwalk, Iowa, for the development of the Echo Valley Community Planned Unit Development (PUD).

Individual parcels within the PUD may be developed independent of other parcels, provided minimum requirements are met, unless modified herein, and the development of the parcel allows for the proper development of adjoining parcels. It is recognized, shifts or modifications to the Master Plan layout may be necessary and compatible with the need to acquire workable street patterns, grades and usable building sites, but the Master Plan layout, including the relationship of uses to each other and the relationship of land use to the general plan framework, and development requirements shall be used as the implementation guide. All modifications shall be made pursuant to Subsection 17.10.170.7 of the Municipal Code.

SECTION 5. GENERAL CONDITIONS. The following general site development criteria shall be integrated into and made a part of the development criteria for the Echo Valley Community Planned Unit Development.

1. All subdivisions, public streets, public street rights-of-way and general development shall adhere to the standards and design criteria set forth in the Norwalk Subdivision Ordinance and the Des Moines Metropolitan Design Standards as adopted or as amended by the City of Norwalk, pertaining thereto, unless otherwise stated within this Ordinance.
2. Any regulation, standard, provision or requirement that is not specifically addressed within this document that is regulated elsewhere in the Municipal Code of Norwalk, the requirements of the Municipal Code shall be enforced.
3. Throughout the PUD, the compatibility of certain project features will be maintained so that the different parcels and densities will all relate to each other and create a sense of a common overall community. Those features will include architectural character and unifying treatment of roadways and curbs, signage, landscaping, lighting and a common pedestrian orientation.
4. Transitioning and buffering of land uses and residential densities shall be addressed as time of site plan or plat approval. A transfer of density up to 20% may be approved through staff not including parcels A, B, or E. Any greater transfer of density or a change of land use shall require an amendment to this Ordinance and the master Plan for the PUD, pursuant to Subsection 17.10.170.7 of the Municipal Code.
5. No building shall be erected within twenty-five (25) feet of any major drainageway, storm water detention basin, or pond areas subject to flooding, or area designated to be within the 100-year zone of Middle Creek or other tributary. This project and all proposed residential plats are subject to review by the Iowa Department of Natural Resources for FEMA Compliance.
6. All subdivisions and streets will adhere to the standards and design criteria set forth in the Norwalk Subdivision Ordinance and the Des Moines Metropolitan Design Standards, as adopted or amended by the City of Norwalk unless noted otherwise in this Ordinance.

The major entry street, is a four-lane divided boulevard at its entrance into the project. This street tapers to a 31' wide minor collector. Due to the unique development and its dependence on limited street connections, all critical street segments shall have a street width of 31' including the primary east-west street through parcel F. All other street right-of-way and paving widths shall adhere to the following standards recommended by the City's Comprehensive Plan for specific street classifications, which shall be determined at the time of platting based on the street's function and projected traffic volumes on the street.

Cul-de-sac streets for Parcels A, B, D, and E shall be allowed to be over 600' in length provided that the "throat" of the cul-de-sac street is constructed as a 31' wide street or boulevard with two 16 ft. lanes between the loop and the base of the cul-de-sac "bulb" or loop, or if no loop exists, to a point that is 600 feet from the terminus or cul-de-sac.

All lots used for single family residential construction shall have direct street frontage. A few exceptions may be provided where street construction is prohibitive due to topographic constraints. The unique terrain and unique development may allow for the use of flag lots in very limited cases within parcels A, B, D. To safeguard the public and future property owners, public services will need to be extended to the lots including water and adequate fire protection. The easement of access or stem of the lot may only serve one lot, have a length of no more than 100 feet and width of no less than 40 feet at the right-of-way line.

Any lots where the dwelling is located more than 200 feet from the street right-of-way or located on a flag lot, shall have a vehicular turnaround located on the end of the driveway. Such turnaround shall have a similar turning radius as a cul-de-sac.

7. The landscape element of the Master Plan identifies proposed open spaces and recreational areas and any environmentally sensitive areas that should be protected and preserved as part of the uniqueness of the area. A street tree planting plan shall be prepared and implemented with a variety of trees as the PUD is developed. Open space is designed extensively into the development to provide the identification and the utilization of the golf course, drainage ways, and utility corridors as passive open space and recreation areas.
8. Lighting within the PUD will be incorporated along the internal residential streets as "theme" lighting for the entire development. The "theme" lighting could be used for multi-family residential, commercial retail, and office projects to encourage continuity from one area to the other.

Up-lighting for landscape material will be incorporated into the entrance designs and shall be designed so as to not direct light into vehicles within the public right-of-way. Detailed lighting plan for both city streetlights and supplemental "theme" lighting shall be submitted to the City and installed at the time of site plan and plat approval.

SECTION 6. LAND USE DESIGN CRITERIA. In addition to the General Conditions set forth within Section 3 herein, the following land use design criteria shall apply to each development area designated by parcel on the Master Plan. The Master Plan document, which is made a part of this Ordinance per Section 4 of this Ordinance, delineates 12 parcels of the PUD, each denoted with a specific parcel number. The parcels hereinafter referred to as "Parcels", are identified for application of specific standards for land use and the development regulations. Interconnecting all the parcels and integrating within them are the streets,

pedestrian walkways/bikeways, and the golf course. Each parcel will be designed, submitted, and then evaluated by the Plan and Zoning Commission and City Council to bring a continuity of the total PUD in design characteristics, separation and transition of land uses, and flow of traffic and pedestrians.

LAND USE AND DENSITY SCHEDULE					
Parcel#	Land Use/ Zoning	Density	Area/Acres	#Units	Density DU/Acre
Parcel A	SFR R-1(100)	1DU/Acre	50.09	50	1
Parcel B	SFR (80)	1.5 DU/Acre	28.30	43	1.5
Parcel E	SFR (80) Only residential uses	2.5 DU/Acre	28.44	71	2.5
Parcel C	Medium Density R-3	6 DU/Acre	15.10	91	4
Parcel D	Multi Family Res. R-4	10DU/Acre	8.94	89	10
Parcel F	SRFR-1(70) Only residential use	4DU/Acre	41.15	165	2
Parcel H	50% C-2 commercial, 50% Office and/or PC, or Limited IC	N/a	35.93		
Parcel G	Commercial/Office High Density	10DU/Acre	7.55	76	10
Parcel I (Greens of E.V)	Medium Density R-3	3 DU/Acre	10.82	32	3
Parcel K	Existing Country Club	N/a	229.21		
Parcel L	New 9 holes Passive open space		87.45		
Parcel J (mix)	Mix including 35% R-3, 35% R-4, 10% limited C-1, 10%CO or PC, and 10% public open space.	R-3 10DU/ac R-4 16DU/ac	21.01	R-3: 73 R-4: 118 More units may be added as more land is assembled	For entire area 9 Du/Acre
TOTAL				808 dwelling units	

BULK REGULATIONS							
Parcel#	Lot Area	Lot Width	Yard				Height
	Minimum Sq. Ft.	Feet	Front Feet	Side Feet	Rear Feet	Golf Feet	Feet
Parcel A	15,000	100'	40'	25' total	40'	50'	35' or 40'
Parcel B	10,000	80'	35'	20' total	35'	50'	
Parcel E	10,000	80'	35'	20' total	35'	50'	
Parcel C	Lots or proj. clust	50' or project	25'	0' lot line or 30' project	30'	50'	35'
Parcel D	Plan for R-4	200'	40'	30' boundary 20' between buildings	35'	50'	35' to max of 125'
Parcel F	8,750	70'	30'	15' total	35'	50'	35'
Parcel H	80,000 complex or 20,000 indiv. pads	200'	50'	10'	50'	50'	Unlimited or restricted by Airport overlay 17.20.050
Parcel G	Commercial See parcel G R-4		50 40	50' 20' 20' between buildings	50 35	50' 50	35'
Parcel I (Greens of EV)	10.82 acres 3,570 sflot	N/a	Already established (approved plat)				35'
Parcel J	R-3 R-4 C-O/PC C-1	50' or project 200' 100' 100'	25' 40' 30' 30'	0' lot 30' project 30' bound 10' 10'	35' 30' 35' 35'	50' 50' 50' 50'	35' 35' 35' 35'
Parcel K/L	N/a	N/a	N/a	N/a	N/a	N/a	N/a

SPECIFIC INFORMATION NOT IN TABLES

1. PARCELS A, B, E. Detached accessory floor may be allowed up to 1,200 sq. ft. before the application of a special use permit is required. The accessory building may not exceed principal structure in floor area. In order to have the full number of permitted units, Parcel E must have an emergency access to 80th Avenue.
2. PARCEL C. For "cluster home developments project proposals" see exhibit "D"
3. PARCEL D. All buffering shall meet the requirements of the zoning ordinance. Where windows or doors face the wall of another building there

shall be a minimum open space separation of at least 30 feet between the two buildings.

4. PARCEL G. All buffering shall meet the requirements of the zoning ordinance. Where windows or doors face the wall of another building there shall be a minimum open space separation of at least 30 feet between the two buildings.
5. PARCEL H. Buffering Limited IC (17.10.150.2) include: A, B, D, E, G, H and J. All buffering shall meet the requirements of the zoning ordinance. Where windows or doors face the wall of another building there shall be a minimum open space separation of at least 30 feet between the two buildings.
6. PARCEL I. Existing development shall adhere to the approved final plat that was fined with Warren County in 1986. Any re-subdivision of the parcels shall adhere to the Norwalk Zoning and Subdivision Regulations.
7. PARCEL K & L. There shall be no principal buildings within the floodplain for Middle Creek. Accessory Structures or uses may require the approval of a flood plain development permit.
8. PARCEL J. This is considered a mixed use parcel and may incorporate additional land in the future. In order to develop, defined development guidelines and a redevelopment plan for this parcel will need to be prepared including acquisition or partnership of properties, detail of and transition of uses, bulk regulations, vacation of ROW, and necessary easements. Such redevelopment plan shall require an amendment to the PUD. The overall land use breakdown shall be the following percentages: 35% HDR, 35% MDR, 10% CO or PC, 10% limited C-1. 10% public open space/park (city vacation of Masteller/E. Wakonda right-of-way).

SECTION 6. PHASING. See Exhibit "B"

SECTION 7. PLATTING REQUIREMENTS. The splitting of any lot within a Parcel of the PUD shall require the submittal of a subdivision plat for review and approval by the City of Norwalk prior to, or in conjunction with development of that portion of the PUD. Said Parcel shall be platted in accordance with the City's Subdivision Ordinance and the Des Moines Metropolitan Design Standards as adopted by the City of Norwalk to delineate within a Plat the lot to be developed or sold separately or any portion there of. Should the planning and platting of the property create common or community wide usage of a private street, drive, parking lot, utility, or common area, a property owner's association document, easement or similar instrument, shall be submitted to the City for review. Said document shall address on going usage, maintenance and upkeep of the shared drives, parking lots, utilities or common areas, and recorded at the County Recorders Office.

SECTION 8. STORM WATER MANAGEMENT. There shall be submitted to, and approved by the City of Norwalk, a complete storm water management plan for each Parcel of the PUD to be prepared by the Developer prior to development within a Parcel.

- SECTION 9. STREET RIGHT-OF-WAY. Adequate street right-of-way shall be provided for the construction, reconstruction and widening of adjoining streets adjacent to, or within the PUD. Said right-of-way shall be dedicated to the City at the time of platting.
- SECTION 10. BUILDING RESTRICTIONS, EASEMENTS AND COVENANTS. Where the City deems appropriate, the landowner shall on each subdivision, adopt building restrictions, easements and/or covenants in a form approved by the City.
- SECTION 11. SOIL TEST REQUIREMENTS. If required, the developer shall be responsible for supplying to the City, information prepared by a qualified soils engineering firm, indicating that existing soil conditions are adequate in stability and strength for construction of public and private improvement. If determined by the soils engineering firm that adequate soils conditions do not exist, the developer shall indicate what measures shall be taken to achieve adequate soil strength and stability for both public and private improvements.
- SECTION 12. TRAFFIC STUDY. A traffic study has been prepared for this project by Howard R. Green Company. Such study is incorporated and a part of the overall project.
- SECTION 14. DEVELOPMENT AGREEMENTS. With the complexity and scale of the master plan, development agreements may need to be developed for a number of aspects including specific development and public improvements including proportional share of off-site improvements that are attributable to this project.
- SECTION 15. STREET NAMES/ADDRESSING. All street names shall be determined at the time of platting. Street names stated in this ordinance are in reference to street names noted on the Master Plan. A general addressing scheme shall be developed for the project and approved by both the Fire and Police Departments. Detailed addressing shall take place at the time of the final plat.
- SECTION 16. DEFINITION. The term "Developer" for the purpose of the Ordinance, shall mean any person, individual, firm, partnership, association, corporation, estate, trust, entity, or agent or same acting or proposing to subdivide land or develop a parcel of land for the construction of a building or buildings.
- SECTION 17. VIOLATIONS AND PENALTIES. Any person who violates the provision of this Ordinance upon conviction shall be punished as set forth in the Municipal Code of the City of Norwalk, Iowa.
- SECTION 18. OTHER REMEDIES. In addition to the provisions set out in Violation and Penalties Section herein, the City may proceed in law or equity against any person, firm or corporation for violation of any section or subsection of this Ordinance.
- SECTION 19. REPEALER. All ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.
- SECTION 20. SEVERABILITY CLAUSE. If any section, provision, or part of this ordinance shall be adjudged invalid or unconstitutional such adjudication shall not affect

the validity of the ordinance as a whole or any section, provision or part thereof not adjudged invalid or unconstitutional.

SECTION 21. EFFECTNE DATE. This Ordinance shall be in full force and effect after its passage, approval and publication as provided by law.

Passed by the City Council on the 5th day of June, 2003.

Jerry Starkweather
Jerry Starkweather, Mayor

ATTEST:

Joyce Cortum
Joyce Cortum, City Clerk

1st Reading: s./Q3
2nd Reading: 5-15-00
3rd Reading: to.s.D3

Passed: _____
Signed: _____
Published: _____

ROLL CALL VOTE:	1st Reading		2nd Reading		3rd Reading	
	Aye	Nay	Aye	Nay	Aye	Nay
Wahl	<u>✓</u>		<u>✓</u>		
Greteman	<u>✓</u>		<u>✓</u>			
Hixenbaugh			<u>✓</u>			
McClarnon	<u>✓</u>					
Lankford			<u>7-</u>			

Exhibit "A"

A portion of the SW 1/4 of the SW fractional 1/2, and the South 2 acres of the NW 1/4 of the SW fractional 1/2, all in Section 6, Township 77 North, Range 24 West of the 5th P.M., Warren County, Iowa being described as follows:

Commencing at the SW corner of the SW fractional 1/2 of said Section 6; thence N86°37'30"E along the south line of the SW fractional 1/2 of said Section 6, 49.7 feet to a point on the east right-of-way line of Highway 28, as it is presently established and to the point of beginning; thence N00°00'45"E along the east right-of-way line of said Highway 28, 27.2 feet; thence N86°40'45"E, 132.0 feet; thence N00°00'45"E, 727.0 feet; thence N90°00'00"E, 18.3 feet; thence N00°00'00"W, 428.5 feet; thence S90°00'00"W, 147.8 feet to a point on the east right-of-way line of said Highway 28; thence N00°18'00"E along the east right-of-way line of said Highway 28, 201.4 feet to a point on the north line of the south 2 acres of the NW 1/4 of the SW fractional 1/2 of said Section 6, said point being the southwest corner of Lot I, Echo Valley Estates, an official plat; thence N87°49'41"E along the south line of Lots I, 2, 3, 4, 5, 6, 7, and 8, all in said Echo Valley Estates and along the north line of the south 2 acres of the NW 1/4 of the SW fractional 1/2 of said Section 6, 1291.13 feet to a point on the east line of the NW 1/4 of the SW fractional 1/2 of said Section 6, said point being the northeast corner of the south 2 acres of the NW 1/4 of the SW fractional 1/2 of said Section 6; thence S01°44'21"W along the east line of the NW 1/4 of the SW fractional 1/2 of Section 6, 66.00 feet to the SE corner of the NW 1/4 of the SW fractional 1/2 of said Section 6, said point being NE corner of the SW 1/4 of the SW fractional 1/2 of said Section 6; thence S01°03'35"W along the east line of the SW 1/4 of the SW fractional 1/2 of said Section 6, 1300.14 feet to the SE corner of the SW 1/4 of the SW fractional 1/2 of said Section 6; thence S86°37'30"W along the south line of the SW fractional 1/2 of said Section 6, 1269.96 feet to the point of beginning.

And

The Northwest fractional 1/2 of Section 6, Township 77 North, Range 24 West of the 5th P.M., except Echo Valley Estates, an official plat, Warren County, Iowa;

And

The Southeast 1/2 of said Section 6, Township 77 North, Range 24 West of the 5th P.M. Warren County, Iowa, except the South 450.43 feet of the East 326.94 feet thereof;

And

The West 1/2 of the Northeast 1/4 of said Section 6, Township 77 North, Range 24 West of the 5th P.M., Warren County, Iowa;

And

The East 1/2 of the Northeast 1/4 of said Section 6, Township 77 North, Range 24 West of the 5th P.M., Warren County, Iowa; except that portion annexed into the City of Des Moines.

ECHO VALLEY COMMUNITY
PLANNED UNIT DEVELOPMENT
NORWALK, IOWA

INTRODUCTION

Echo Valley Community is over 500 acres of Planned Development with a vast majority of the land devoted to recreation and residential uses. The early vision of Eddie Coppola is evident by the rolling hills, scattered woodland, open fields, and natural creeks and ponds. It is perfectly suited for a golf course community.

The existing Echo Valley Country Club is a full-service private club with all of the recreational facilities expected 18 holes of golf, tennis, swimming, and major banquet facilities for weddings and large parties.

THE SOUTH DEVELOPMENT

The new golf facilities include an additional 9 holes of golf and a clubhouse. The new course wraps around the natural topography and rich tree cover in a figure-eight pattern. In the first phase development over 75% of the homes will be on the golf course or have breathtaking views of it.

This south central part of the Planned Unit Development is designed for large lots of one acre or more. This will provide executive settings for luxury houses. Golf course views are virtually all around this neighborhood. To the south, one-half acre lots are provided with the same visual access to the course. Just to the east of the neighborhood are the up-scale townhouses. This setting, just off the main entry, is convenient and has extensive visual access to the course.

The west corner of the project is a mixed-use development of residential, commercial and office uses. Again, there is great visibility to the open space and golf course all along the east border.

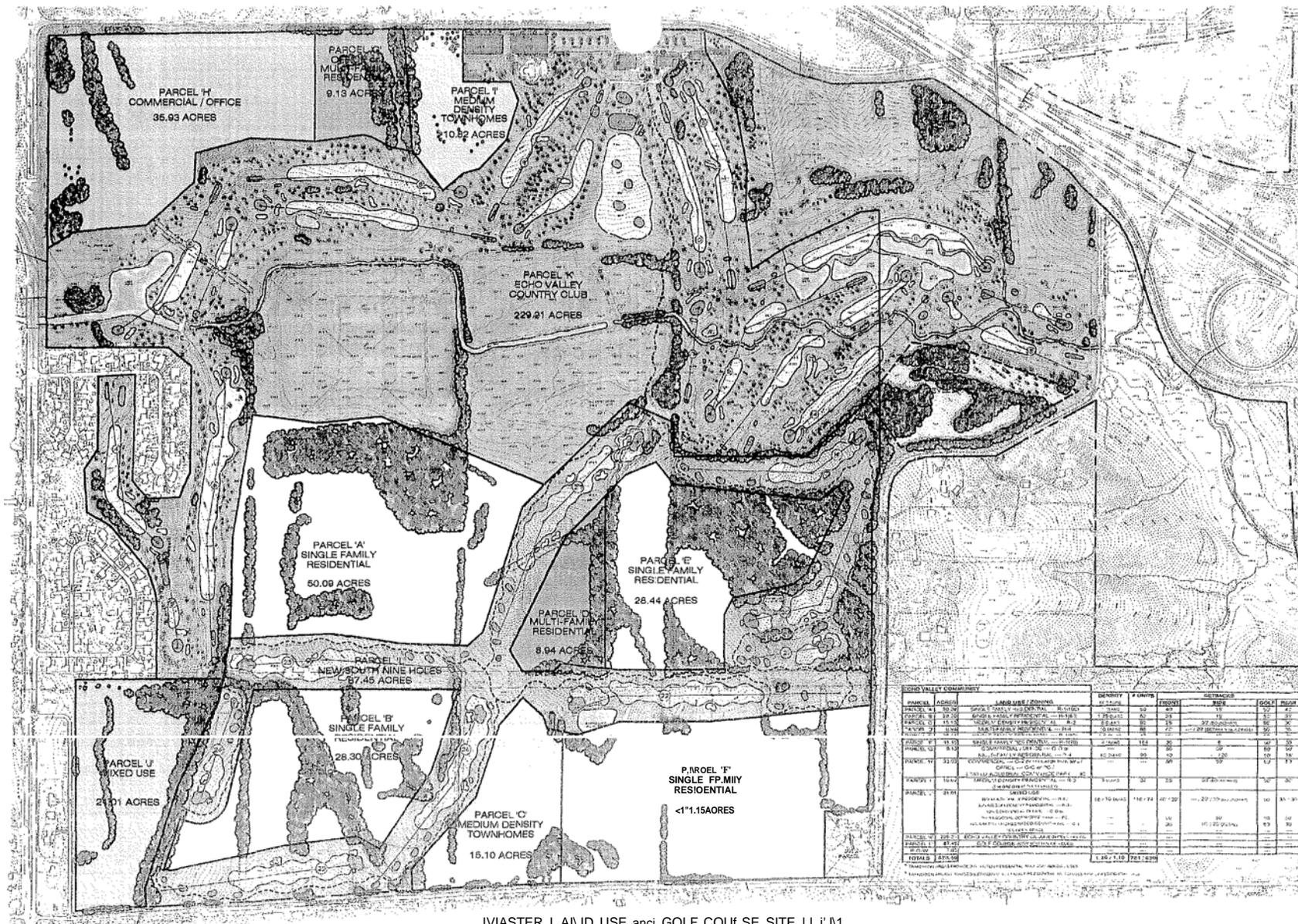
NORTHWEST DEVELOPMENT

Finally, the northwest corner is envisioned as a mixture of commercial, office, light industrial, and multi-family. The intent is to keep this mixture flexible to allow the market to determine the percentages of each use. Until more population base occurs throughout Norwalk, this area will need time for development.

DESIGN APPROACH

At Echo Valley Community, overall quality and aesthetic harmony will be maintained while allowing for individual expressions and tastes one would expect in a custom-home community. All homes in Echo Valley will be built with a strong emphasis on maintaining architectural integrity. While a custom home ultimately is the expression of the homeowner's taste and style, it is imperative in a community of this caliber that the housing styles complement the neighboring homes and overall appearance of the community.

The theme and quality of the community is established at the Beardsley Street entrance with extensive landscaping and carries through with custom-designed neighborhood entrances, street signs, street lighting and even custom-designed mailboxes. Careful attention to landscaping of curbs and key intersections in the community provides a style of living that parallels the metropolitan Des Moines. The goal is to create this elegance in a very casual manner.



MASTER LAND USE and GOLF COURSE SITE PLAN
The SOUTH NINE HOLES at ECHO VALLEY COUNTRY CLUB

ECHO VALLEY COMMUNITY

NORWALK, IOWA



ENVIRONMENTAL DESIGN GROUP, LTD.
ARCHITECTS - LANDSCAPE ARCHITECTS - ENGINEERS
500 Walnut Park, East Hill, Norwalk, Iowa 50551 (515) 281-4200
P: 515-281-4200 • F: 515-281-4201 • www.edg.com

Exhibit "D"

"Detached" Cluster Home Development Guidelines

- a. Minimum setback from all boundaries of the site shall be at least 30 feet. No structures including decks, patios, or other ground structures shall project into this area. Exceptions to this provision would be any association owned entry feature including signage and landscaping.
- b. Minimum distance between buildings shall be at least 10 feet (roof overhangs, fireplaces, etc. may encroach up to 2 feet)
- c. The maximum density for these parcels will be 6 du/acre.
- d. The owner of the cluster-home will also own the lot beneath the dwelling unit.
- e. The minimum lot area for the dwelling unit shall be 1,400 square feet and the minimum width 20 feet.
- f. The parking per unit shall be at least 2 in the garage and 2 in driveway for a total of 4.
- g. Additional overflow parking will be required in small parking areas spread evenly throughout the site. The site shall provide at least 1 parking stall for every five dwelling units.
- h. The minimum driveway dimensions shall be 16 feet in width and 25 feet in length.
 1. The private streets serving the cluster-homes shall be at least 22 feet wide.
- J. Minimum amount of common open space for the complex shall be 30 percent.
- k. No buffer will be required for these parcels since they will developed into individual detached units and extensive landscaping and berming techniques will be utilized to enhance the sites.
 1. All accessory structure or uses as permitted within Section 17.10.050.3 of the Norwalk Zoning Ordinance are permitted.

USES ALLOWED IN THE C-2 DISTRICT

All C-O uses:

A. Professional and semi-professional office buildings for the following:

1. Abstract title
2. Accountants and Bookkeeping
3. Actuaries
4. Advertising (no shops)
5. Adjusters (insurance)
6. Aerial survey and photography
7. Appraisers - no sale or rental of any type of merchandise or equipment
8. Architects
9. Attorneys
10. Auditors
11. Banks and financial institutions
12. Business analysts - counselors or brokers
13. Building contractors, office only (no shops or storage)
14. Chiropractors
15. Consulates
16. Counseling, child guidance and family service
17. Court reporter and public stenographers
18. Credit reporting
19. Dentists
20. Detective agencies and investigating services
21. Drafting and plan services
22. Engineers, professional
23. Insurance and bonds
24. Manufacturer's agents
25. Market research
26. Medical doctors and practitioners
27. Model agencies or schools
28. Mortgage brokers
29. Notary public
30. Opticians
31. Optometrists
32. Podiatrists
33. Public libraries
34. Public relations
35. Radio and television production and broadcast studios
36. Real estate
37. Real estate management
38. Secretarial services
39. Shoppers information services
40. Social service bureaus
41. Stock broker exchanges, investment services
42. Tax consultants
43. Telephone answering services
44. Theater ticket agencies
45. Travel agencies
46. Zoning consultants

- B. Clinics or group medical centers, including dental clinics, but not including animal clinics.
- C. Day care center, day nursery or nursery school, licensed in the State of Iowa, provided no building, structure, or accessory use for property so used is located less than twenty-five (25) feet from any other principal building on any other lot in an R District; and provided there is established and well maintained in connection therewith a completely fenced play lot of no fewer than one thousand (1,000) square feet in area for the first twenty (20) or less children under care, with twenty-five (25) square feet added to such play lot area for each additional designated child capacity of the principal building.
- D. Public and parochial schools (elementary and secondary), colleges and universities.
- E. Professional business training school, whose functions are wholly contained within the structures, or otherwise effectively conceals its functions from visual, olfactory, or auditory observation outside the premises.
- F. Health and athletic fitness centers and clubs with all indoor facilities.
- G. Church, chapel, temple, synagogue and similar place of worship, and associated residence of clergy or ordained official of the religious organizations.
- H. Funeral homes and mortuaries.
- I. Buildings and uses owned by a county, city and county, city or other political subdivision which are operated for the social benefit or convenience of the public, but excluding equipment storage yards and garages which are operated and maintained for the necessary business and industrial service of the community.
- J. Museums and libraries not operated for profit.
- K. Swimming, athletic, and tennis clubs or country clubs and similar public and privately owned uses with outdoor facilities, by Special Use Permit.

All C-1 uses:

The following neighborhood retail commercial and service establishments and uses:

1. Apparel (specialty) shops
2. Bakeries (retail), including baking for sale on premises
3. Beauty shops and barber shops, but not including schools
4. Book stores and stationery stores
5. Camera and photographic supply stores, including photo finishing services for the general public
6. Candy, nut, and confectionery stores
7. Coffee shops
8. Coin operated laundries and dry cleaning establishment using nonflammable solvents

9. Convenience store; provided fuel sales facilities have a minimum separation of one thousand (1,000) feet between similar businesses with fuel sales and there is no vehicle washing or repair on site
10. Dairy product and ice cream stores and parlors; retail over-the counter sales only
11. Drug stores
12. Florists, including potted house plants
13. Gift, novelty, and souvenir shops
14. Hobby and game shops
15. Home accessories, such as glassware and linens
16. Home video equipment and movie rentals
17. Jewelry stores
18. Liquor stores
19. Meat and seafood specialty shops
20. News dealers and newsstands
21. Optical goods and hearing aids
22. Restaurants and cafes, provided there is no drive-through or drive-in facility
23. Retail coin, philatelist, and autograph shops
24. Sewing, needlework, and piece goods stores
25. Shoe repair shop, shoe shining
26. Tobacco stores
27. Tailor shops, including clothing alteration and repair shops
28. Watch, clock and jewelry repair
29. Any use which is found by the Zoning Administrator to be similar to one of the above named uses, and in his/her opinion, conforms to the intent of this section.

All C-2 uses:

The following retail commercial and service establishments and uses:

1. Antique stores, but not including refinishing or refurbishing.
2. Artists' and architectural supply
3. Automobile parts store; no repair work on site permitted.
4. Banks, savings and loans and other financial institutions with drive-in facilities and free-standing ATM machines permitted
5. Bars and night clubs
6. Billiard parlors and pool halls
7. Bowling alleys
8. Clothing and accessory stores, including storage and repair of fur garments, but not including trading in furs
9. Commercial art galleries
10. Computers, typewriters, copiers, and similar office equipment retail sales and service
11. Convenience stores
12. Construction Contractor's Office, with up to 90 percent of the building devoted to interior storage of materials, tools, and equipment. No external storage of materials, tools, or equipment shall be permitted
13. Department or variety stores
14. Electrical repair shops

15. Floor covering stores, primarily engaged in retail sales and incidental installation, but not including establishments primarily engaged in installing or supplying building contractors
16. Formal wear and costume rental
17. Fruit stores and vegetable markets, provided that no outdoor or open-air display, sales, or storage shall be permitted except by special use permit
18. Grocery and food stores
19. Hardware stores
20. Hospitals
21. Hot tub or sauna sales, but not including swimming pools
22. Hotels and motels
23. Household appliance stores
24. Household furniture, retail sales but not including cabinets
25. Household improvement products stores, i.e., paint, glass and wallpaper stores, retail sales to the general public only
26. Ice and roller skating rinks, indoor only
27. Interior decorations, including retail sales of draperies and curtains
28. Luggage and leather goods
29. Music or dance schools or studios, including children's or amateur instruction and exercise classes, but not including ballrooms or dance halls
30. Office furniture, and supplies, retail sales only
31. Pet shops, but not including boarding or outdoor kennels
32. Postal service (local substation of United States) or private parcel post delivery service
33. Radio, television, and music stores
34. Restaurants, drive-in facilities permitted
35. Sporting goods stores and bicycle shops, but not including sales of motorized vehicles
36. Swimming, athletic, and tennis clubs or country clubs, and similar public and privately owned uses with outdoor facilities
37. Theaters; does not include drive-in theaters
38. Toy stores
39. Veterinarian clinics for household pets on an out-patient basis only; no overnight boarding or lodging except by special use permit
40. Any use which is found by the Zoning Administrator to be similar to one of the above named uses, and in his/her opinion, conforms to the intent of this section.

USES ALLOWED IN THE C-3 DISTRICT

All uses in the C-O, C-1, and C-2, plus:

The following retail, commercial, and service establishments and uses:

1. Amusement Parks, and outdoor stadiums and arenas.
2. Automobile, trailer, motorcycle, boat, and farm implement establishments for display, hire, rental, and sales (including sales lots); including all repair work in connection with personal or customers' vehicles
3. Carpenter and cabinetmaking shops
4. Car Washes, including self-service
5. Gas Stations or Service Stations
6. Lumber yards, retail only
7. Monument sales yards
8. Public auction buildings or rooms. (Does not include animal, vehicle auctions.)
9. Mini-Warehouse
10. Transportation terminal or truck stops, including minor repairs as accessory use
11. Small repair shop, including but not limited to bicycle, motorcycle, lawn mower, and garden tractor repair. All activities must be confined inside the building(s), including storage of parts and machines.
12. Any use which is found by the Zoning Administrator to be a use similar to one of the above named uses, and in his/her opinion, conforms to the intent of this section.

Healthy Neighborhoods and Healthy Streets

Their design and effect on safety, environment and first responder times

By Dan Burden, Principal with Glatting Jackson Kercher Anglin, Co-Founder of Walkable Communities

Compact Neighborhoods, Healthy Streets

Livability, affordability, sustained home values, environmental issues incident response times and “deployment” are linked to neighborhood and street designs. In general, neighborhoods with higher connectivity (more blocks and intersections per square mile) provide easier access, are safer, have higher rates of walking, are more transit friendly and more sustainable. Meanwhile, streets with lower lane and curb-to-curb widths are more safe, affordable, sociable, economically sound and environmentally friendly. *Healthy Streets* assure low speeds and volumes increase walking, bicycling and socializing. To do this *Healthy Streets* must be part of a well-connected street system. These street designs are not for developers failing to apply Smart Growth, sustainability, and well integrated street systems. A combination of market forces, geography and other issues call for the widest possible selection of street options. These options call for more tools.

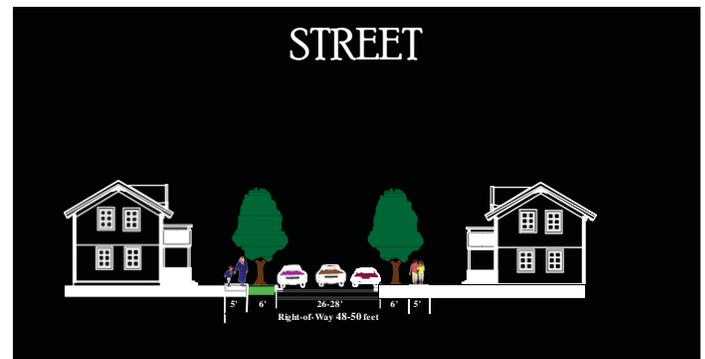
Correctly designed healthy streets protect access, provide movement of large equipment and support deployment of equipment. However, attention to design details is essential.

Basic features of these *streets, lanes, alleys and avenues* are summarized in this section. Block entry turn radii, (preventing parked cars from blocking access near corners), thoughtful spacing of trees, saturation levels of on-street parking, widening on curves, even driveway placements are among the complexities requiring choices and precise design details.

Emphasis is placed on keeping designs flexible. Being too prescriptive creates problems for developers, designers and responders. Thus, a focus on adherence to performance (not prescribed numbers) is stressed in these pages. Performance measures keep streets flexible in their design; meeting the widest range of uses and address complex home buying markets. Presence of trees, on-street parking, curves, block length, terminating vistas and street connectivity are a few elements influencing motorist speed.

Healthy street designs for local, collector and arterial streets must provide each of the following: (1) assure large equipment access and movement, (2) provide appropriate speed and volume, (3) allow motorists to pull over to let responders by, and (4) allow sufficient width for incident “deployment” (generally 16-20 feet).

Safety. Studies by Swift, Noland and Dunbaugh (among others) point out how better connected street systems and narrower streets and lanes (generally 26-28 foot wide local streets or 9-10 foot lanes (for Avenues) are the most safe.



Images from top to bottom: *One of Chico, California's most loved streets is 24 feet wide with parking on both sides. In order for this street to meet fire access and operations needs (1) entries must protect access with protected entering radii and (2) street parking cannot be saturated. A plan view provides two models: (1) Left, traditional streets with 26-28 foot width protects access and provides a 20 foot clear zone midblock, (2) Conventional streets protect access but keep 20 foot clear the entire length. Bottom photo shows how use of driveways allows operations and access on a 28' wide street.*

For local streets, curb-to-curb widths of 26-28 foot create the greatest livability, walkability and safety, especially when parking is included on each side of the street. Also, as a general rule, the fewer the number of lanes in a neighborhood collector or arterial road, the lower the speeds and the safer the roadway to travel along or to cross.

Use of Minimums. Minimum recommendations shown in text below can be exceeded, still providing safety and livability. Beyond a certain range, however, design interventions must be added. Otherwise, significant safety and other values are lost. In the scene to the right narrow streets with parking on both sides create access issues on curves. Widening streets through curves, or parking removal from one side addresses this problem.

Local Streets, Option One. The safest local street design is a “yield street”, where one motorist pulls over, allowing the opposing to continue. Yield streets work well when street connectivity keeps traffic volumes to 400 or fewer cars per day. (.6 cars per minute). Yield streets allow 50-foot R-O-W’s, which are ideal for minimizing hard surface materials, water runoff and other environmental impacts. These dimensions also maximize safety, active transportation, shade energy conservation and socializing. Lane lines are generally not marked on yield streets. When curbs are used, 26-28 foot widths allow 6 feet per side for parking (most cars are 5-6 feet wide) allowing a 12-14 foot space for travel. When parking is moderate or light. Streets as narrow as 24 feet still allow a 12 foot travel lane. If parking is not dense (often assured when alleys are used) there is plenty of space for motorists to pull over. If parking is dense and blocks are long (over 600 feet) parking is restricted to one side, or one-way streets are used.

Access Assurance: Access is assured through use of curb extensions on corners. Use of one or two curb extensions (curb bulbs) narrow entry throats to as little as 14 feet. Properly placed curb extensions push parking back, allowing the largest vehicles easy entries.

Local Streets, Option Two. The second safest street eliminates yield practices, but remains safe and environmentally friendly using short blocks, and narrow 20 foot wide carriage-ways. Parking is inserted between tree wells, spaced each 20 feet. Parking deck materials are permeable, and water can be channeled into swales or rain gardens. An amount of green equal to yield streets is achieved through use of tree wells. A growing canopy keeps speeds low. Many variations are applied, including “permeable curbs” allowing water to flow into retention/absorption areas or other spaces for local water treatment and percolation.

Access Assurance: Access is assured through use of curb extensions on corners. Use of one or two curb extensions (curb bulbs) narrow entry throats to as little as 14 feet. Properly placed curb extensions push parking back, allowing the largest vehicles easy entries. Narrow travel ways of 12 feet also assure people will not park in the lane blocking access.



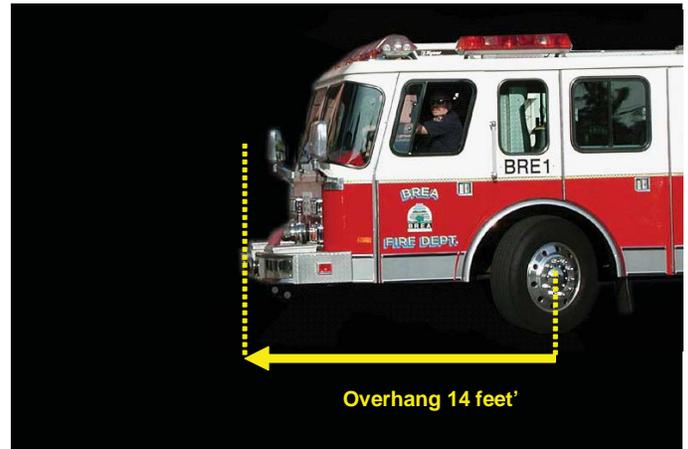
Above: Option One: *Healthy and safe streets must be more precise. Narrow streets must either be widened on curves, or parking must be removed from one side. If block lengths exceed 400 feet and parking is saturated, streets must be one-way, or other provisions (curb extensions or driveway patterns) should create “deployment stations” each 200-300 feet.*

Below: Option Two. *In some cases safe, walkable, sociable and environmentally friendly streets are achieved through alternative surface materials (pavers), use of inset pervious parking, ample tree wells and related measures. Streets can be wider. Both options can be fit in a 50 foot right-of-way.*



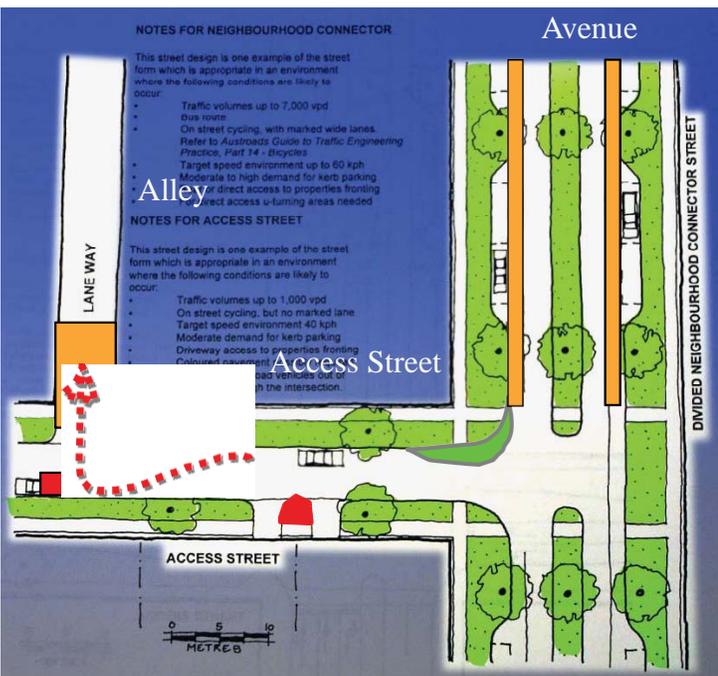
Curb Radii and Midblock Curb Extensions

Access and operations are protected through use of proper width street entries. A combination of curb extensions, sometimes combined with an added "effective radius" from use of bike lanes or inset parking, and other tools assure oversize vehicles gaining entry to neighborhoods. The actual turn radius of fire apparatus must accommodate the front overhang of equipment. Auto-Turn and other engineering tools must be calibrated to local fire equipment. The effective turning radius on equipment is



Tools Assuring Access

Lower Left: Uses of curb extensions, mountable medians, and in select locations mountable curbs (and other tools) are used to prevent parking in unwanted locations, or to otherwise a responder stay in motion. Healthy streets require more tools than wider conventional tools. Upper right: Curb extensions which narrow entries prevent motorists from parking in undesired locations. Bottom right: Correct use of a mountable curb when medians are used.



Lanes. An even narrower travel way than a “street” is a “lane.” Lanes are generally 16-20 feet wide, with parking limited to one side. Lanes are often one-ways but can be two way. Lanes are generally found near parks or parkways, but can be found as short connectors in other locations.

Alleys. Alleys have very low volumes, typically under 200 vehicles per day, or less than one car every two minutes. Alleys with a 12 foot wide paved area minimizes materials and sets a design eliminating motorists from parking and blocking the alleyway. These dimensions required 8 foot building setbacks on each side. This design creates platforms for emergency responders to have a 28 foot operations space. Narrow travel ways of 12 feet also assure people will not park in the lane blocking access.

Access Assurance: Access is assured by dropping curbs on corners and hardening edges, creating a wider effective radius on corners.

Collector or Minor Arterial Streets.

R-O-W as narrow as 60 feet can allow a Complete Street, giving full access to walking, bicycling, and all vehicles, including cars, freight and responders. These streets also allow necessary movement and deployment for first responders. This width still allows for 10 foot turn lanes using crossing islands where needed.

Two 10 foot travel lanes and two 5 foot bike lanes/shoulders are added. The presence of bike lanes creates a wider effective turning radius. Planter strips for trees are limited to 5 feet, and sidewalks are also limited to 5 feet.

When full length medians are desired, bike lanes are widened to 7 feet, allowing motorists to pull over to allow responders to pass.

New roads or re-striped roadways can use reduced lane widths (9-10' lane widths, versus 11'-12'). With narrow lanes motorists tend to lower their speed and remain more vigilant. In combination a slight reduction in crash rates can result. Lane widths of connectors or arterials are striped with 4-6 foot wide bicycle lanes. When bike lanes are not desired the edge line provides paved shoulders of any width. If widths of 6 or more feet can be provided sufficient space is created to allow motorists to pull over to allow fire equipment to get by. These treatments make the driving area appear to be narrow without adding curbing to physically narrow the roadway. The street can also be physically narrowed by extending sidewalks, providing landscaped areas, or adding on-street parking within the former curb lines. This often reduces vehicle speeds along a roadway section and enhances movement and safety for pedestrians. Adding bicycle lanes on higher-volume streets with speeds in excess of 20 mph enhances bicycle travel by increasing the predictability of both car and bicycle movements. Such treatments are particularly desirable for a neighborhood when several streets are treated in this way to create a connected system of bike lanes.





Above: Healthy Streets are part of neighborhoods with connected street systems. Avenues surround this Chico, California neighborhood, providing multiple points of entry. Nineteen entries disburse traffic, keeping intersections free to do their work. High performance avenues allow responders to keep steady movement. This keeps response times low. Upon entering the neighborhood well connected internal streets provide redundant points of access to each property. Easy movement, protected access and assured “deployment” can be planned for both first and subsequent responders. Many state, regional and even local codes currently discourage or dissalow disbursed entry patterns.

Other Photos: A variety of avenue, lane and street types are shown. Developers, designers and responders require maximum flexibility in design. This calls for performance, not overly prescriptive code.



Proposed changes to the National Fire Code

503.2 Specifications. Fire apparatus access roads shall be installed and arranged in accordance with Sections 503.2.1 through 503.2.7.

503.2.1 Dimensions. Fire apparatus access roads shall have an unobstructed width of not less than 20 feet (6096 mm), except for approved security gates in accordance with Section 503.6, and an unobstructed vertical clearance of not less than 13 feet 6 inches (4115 mm).

503.2.2 Authority. The fire code official shall have the authority to require an increase in the minimum access widths where they are inadequate for fire or rescue operations.

503.2.2.1 The fire code official shall have the authority to approve a decrease in the minimum access width when all of the following conditions are met:

1. The street network provides support for the movement and deployment of the emergency vehicles of the local jurisdiction's fire department and emergency medical services.
2. All buildings, including residences, are equipped throughout with an approved automatic sprinkler system installed in accordance with Section 903.3.1.1, 903.3.1.2 or 903.3.1.3.

503.2.2.2. The fire code official is authorized to require the owner or agent to provide, without charge to the jurisdiction, a technical opinion and report to support requests for reduced access widths. The opinion and report shall be prepared by a qualified engineer, specialist, or fire safety specialty organization acceptable to the fire code official and shall include an analysis of the access provisions of the streetscape design, building or premises uses and fixed protection, and recommend approval, denial, or necessary changes.

503.2.3 Surface. Fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall be surfaced so as to provide all weather driving capabilities.

503.2.4 Turning radius. The required turning radius of a fire apparatus access road shall be determined by the fire code official.

503.2.5 Dead ends. Dead-end fire apparatus access roads in excess of 150 feet (45 720 mm) in length shall be provided with an approved area for turning around fire apparatus.

503.2.6 Bridges and elevated surfaces. Where a bridge or an elevated surface is part of a fire apparatus access road, the bridge shall be constructed and maintained in accordance with AASHTO HB-17. Bridges and elevated surfaces shall be designed for a live load sufficient to carry the imposed loads of fire apparatus. Vehicle load limits shall be posted at both entrances to bridges when required by the fire code official. Where elevated surfaces designed for emergency vehicle use are adjacent to surfaces which are not designed for such use, approved barriers, approved signs or both shall be installed and maintained when required by the fire code official.

503.2.7 Grade. The grade of the fire apparatus access road shall be within the limits established by the fire code official based on the fire department's apparatus.

CITYFIXER

10-Foot Traffic Lanes Are Safer—and Still Move Plenty of Cars

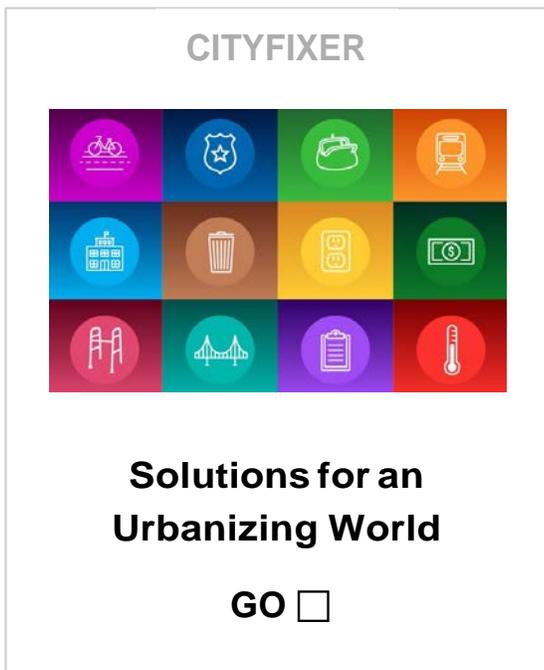
The case against 12-foot lanes in cities, in 3 charts.

ERIC JAFFE | [@e_jaffe](#) | Jul 28, 2015 | [43 Comments](#)



[Raphael Desrosiers / Flickr](#)

At first glance, it makes sense that wider traffic lanes could be safer traffic lanes. Drivers are prone to bad decisions and sleepiness and text messages and fits of rage. Providing some buffer room seems a reasonable way to keep them from veering into anything else sharing the road.



But as Jeff Speck [persuasively argued](#) during our Future of Transportation series, the conventional engineering wisdom that favors 12-foot traffic lanes to 10-foot lanes is deadly wrong—especially for city streets. The problem largely comes down to speed: when drivers have more room, cars go faster; when cars go faster, collisions do more harm. The evidence cited by Speck on the safety hazards of wider lanes is powerful, though to date it remains pretty scarce.

That body of work just got a bit thicker, thanks to a new study by civil engineer Dewan Masud Karim ([spotted by](#) Chris McCahill at the State Smart Transportation Initiative). Evaluating dozens of intersections in Toronto and Tokyo, Karim linked lower crash rates to narrower lanes—those closer to 10- or 10.5-feet wide than to 12-feet. Sure enough, wider lanes meant speedier cars, and yet narrower lanes were perfectly capable of moving high volumes of traffic.

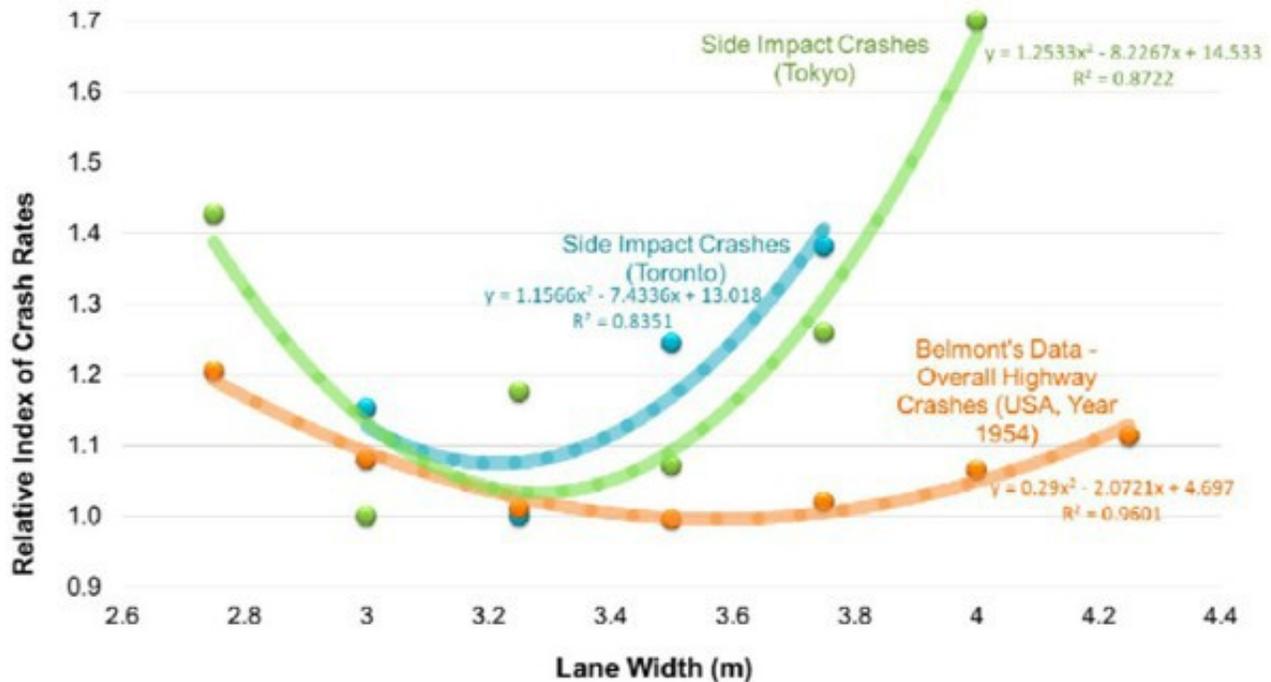
He [concludes](#):

Given the empirical evidence that favours ‘narrower is safer’, the ‘wider is safer’ approach based on intuition should be discarded once and for all. Narrower lane width, combined with other livable streets elements in urban areas, result in less aggressive driving and the ability to slow or stop a vehicle over shorter distances to avoid a collision.

Let’s take a closer, chart-filled look at the details.

Narrow lanes are safer

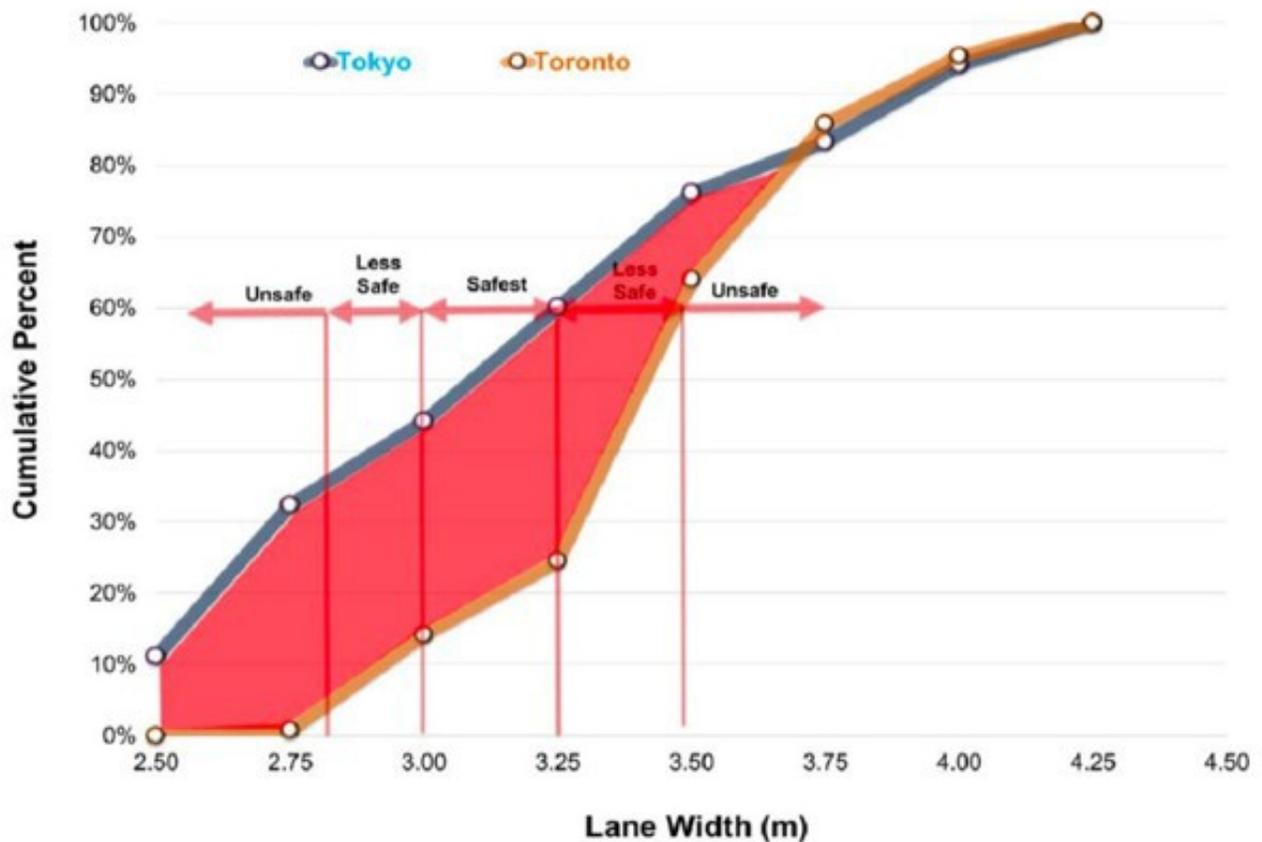
An analysis of several years of crash data in both cities showed a clear sweet spot for lane width around 10.2 feet in Tokyo (3.1 meters) and 10.5 feet in Toronto (3.2 meters). Crash rates increased as lanes got too slim and drivers ran out of space; they also rose as lanes got wider. Karim writes that these results “clearly demonstrate why ‘conventional wisdom of lane width’ does not hold up to scientific scrutiny.”



Crash rates in Toronto (blue) and Tokyo (green) were lowest in lanes between 10 and 10.5 feet wide.

Cars in wider lanes tend to go faster

Generally speaking, traffic lanes in Tokyo are narrower than those in Toronto, with a much greater percentage falling into what Karim calls the “safest” width range. He believes wider lanes, and the faster traffic that comes with them, explains why Tokyo’s collision rates were lower than those in Toronto, despite the fact that Tokyo is a much more populous city with a greater traffic volume. At the time of a collision, the average speed of a car in Toronto was 34 percent higher than it was in Tokyo, according to Karim’s figures.



Tokyo (blue) tends to have narrower travel lanes than Toronto (orange), which might explain why collisions occur there at slower speeds.

Narrow lanes still carry lots of traffic

A common rebuttal to reducing lanes from 12 to 10 feet is that doing so will produce congestion. But smart design can accommodate slim lanes and traffic alike—something New York City recently [discovered](#) when it narrowed car lanes to make way for bike lanes. Karim found that traffic capacity in Toronto was actually highest for lanes right around 10-feet wide.

“Traffic delays on urban roads are principally determined by junctions, not by midblock free flow speeds,” he writes. “Reducing lane width to 3.0 m [~10 feet] in urban environments should therefore, not lead to congestion.”

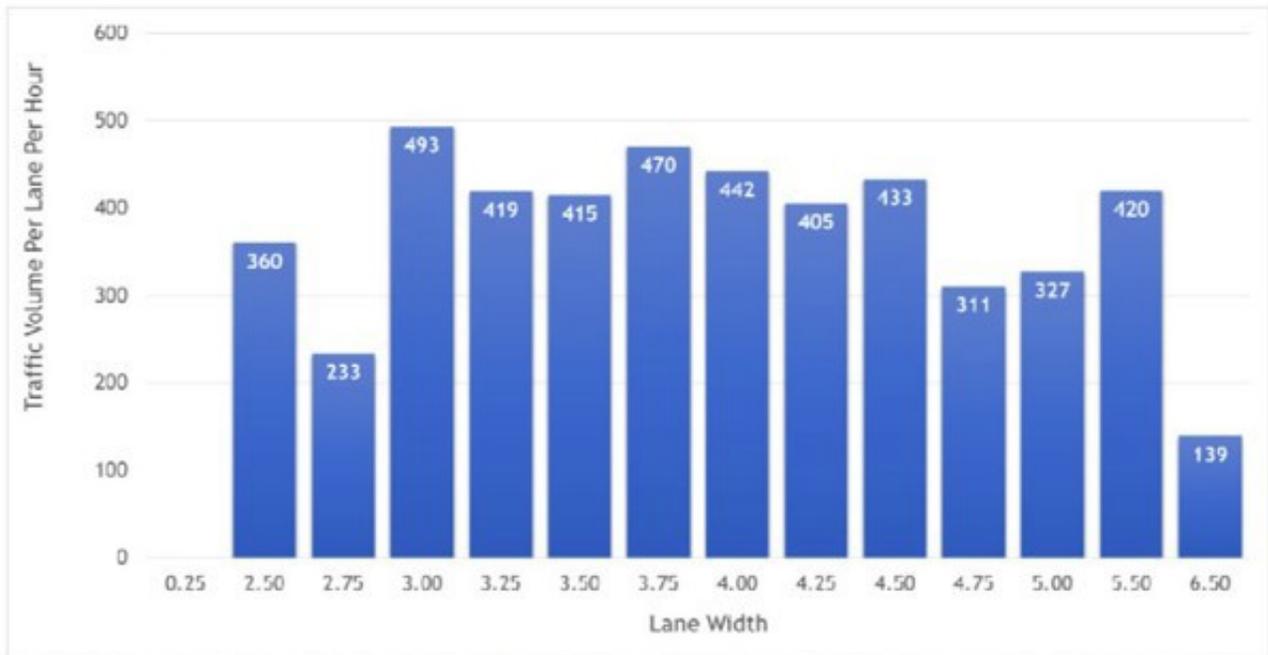


Figure 6: Distribution of traffic capacity (per lane per hour) demand and lane width (Toronto)

Plenty of cars still moved through lanes that were roughly 10-feet wide.

About the Author



Eric Jaffe is the former New York bureau chief for CityLab. He is the author of *A Curious Madness* and *The King's Best Highway*.

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Skinny Streets

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Skinny Streets



Skinny Streets is the name of a movement aimed at reducing the dimensions of streets in municipal standards. Since the 1990's, many cities have revisited their overly wide street design standards and adopted narrower profiles. Reducing the width of streets provides a number of benefits. Skinny streets reduce: speeding, vehicle crashes, street construction costs, pedestrian crossing distances, impervious surfaces (and therefore stormwater drain capacity), street maintenance and resurfacing costs, and heat re-radiation which contributes to the urban heat island effect.

New urbanist traffic engineer James Charlier of Boulder, Colorado, notes that street dimensions from the last century grew increasingly wide to accommodate what are now obsolete requirements: providing room for a four-horse wagon team to make a U-turn, or providing sufficient width for military vehicles to respond to a national emergency. Fire departments, citing public safety concerns, deploy ever longer and wider vehicles and then insist on wider streets to accommodate turns and the passing of two such vehicles on a single street. These single-minded "safety" concerns overlook the increase in crashes, injuries, and fatalities that come with wider street dimensions. They also ignore the steady reduction in house fires that has occurred over the last several decades with the phasing in of better building materials, indoor sprinkler systems, and less frequent cooking.

Defining the Ideal Street

In the early 1990's, pedestrian and bicycle planner Dan Burden worked with a team of traffic engineers to define ideal street dimensions for street types ranging from residential to multi-lane boulevards. Burden and his team examined streets in older, traditional neighborhoods, specifically those that seemed to serve traffic effectively while encouraging low speeds and safety for other users. The results were compiled in a deceptively simple

guidebook entitled *Street Design Guidelines for Healthy Neighborhoods* [1]. The guidebook provides street dimensions for the entire right-of-way, from the outside edge of the sidewalk inward. The recommended street dimensions are narrower and far safer than conventional standards allow. For example, the recommended residential street is 26 feet wide with parking on both sides. By comparison, most city street standards require 36-40 foot widths.

Proliferation of Skinny Street Standards

Municipalities throughout the US, weary of multiple complaints of speeding on residential streets, have replaced their wide street standards with narrower standards. Under the auspices of the [Congress for the New Urbanism](#), architect Donald Cohen assembled a [list of example localities](#) . The state of Oregon has adopted skinny street standards as a recommendation for the entire state. It is important to note that state fire officials were involved in the creation of these standards.



Obstacles to Skinny Streets

A previous major obstacle to adopting narrow street standards -- or perhaps just an excuse -- has been the question of legal liability for municipal traffic engineers who are asked to approve narrow standards. This is because the narrower standards are thought to be in conflict with national recommended standards such as those of the American Association of Highway and Transportation Officials (AASHTO) -- the so-called "Green Book". Courts tend to favor national guidelines over "deviations." However, the Green Book provides a great deal of flexibility, to the point of encouraging narrow widths (e.g., 26') on low-volume residential streets, and traffic engineers are learning that narrow street standards make a great deal of sense in most cases.

By contrast, fire departments present a more formidable obstacle to the adoption and use of skinny streets standards. As Ewing, et al. note, "[t]he main obstacle to skinny streets in the United States is no longer the city traffic engineer, but rather the local fire chief, who enforces the fire code with singular purpose." [2] This is quite unnecessary, since in most cases it can be shown that fire apparatus can usually navigate narrow streets. Where this is in doubt, driving tests can show where parking prohibitions, wider corner radii, or smaller fire equipment can be deployed as a solution. A useful guide for fire departments - or for those working to convince the local fire chief - is Dan Burden's manual on the topic [3].

ALSO ON THE LIVABLE STREETS NETWORK

- [Lane Width](#)
-

REFERENCES

Each source is referred to by the same number every time it is cited. Please keep citation style consistent.

[1] Burden, Dan, with Michael Wallwork, Ken Sides, Ramon Trias, and Harrison Bright Rue. 1999. *Street Design Guidelines for Healthy Neighborhoods*, Local Government Commission Center for Livable Communities.

[2] Ewing, Reid, Ted Stevens, and Steven J. Brown. [Skinny Streets and Fire Trucks](#) (pdf). *Urban Land*, August 2007.

[3] Burden, Dan. 2001. *Emergency Response: Traffic Calming and Traditional Neighborhood Streets*, available as a free download from the [Local Government Commission](#) .

[4]

PICTURE REFERENCES

Pictures are cited in the order they appear above. Please keep citation style consistent.

[1] Kalamazoo, MI. Photo by Dan Burden via the Pedestrian and Bicycle Information Library.

[2] Rt. 62, Hamburg, NY. Photo by Dan Burden

FURTHER READING

- Girling, Cynthia and Kellet, Ronald. 2006. *Skinny Streets and Green Neighborhoods*, Island Press.

- [Charlier and Associates](#), a firm specializing in skinny street and new urban designs.
- [Sierra Club web page on narrow streets](#)
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KEYWORDS

movements, skinny streets, lane width, traffic engineers, emergency response, narrow streets, speeding, traffic calming, safety

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**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 13
For Meeting of 5.5.2016

- REQUEST:** Request from United Properties LC to approve the Preliminary Plat of the Marketplace at Echo Valley
- STAFF CONTACT:** Luke Parris, AICP
City Planner
- APPLICANT(S):** United Properties LC Civil Design Advantage, LLC
4521 Fleur Drive, Suite C 34-5 SE Crossroads Dr. Suite G
Des Moines, Iowa 50321 Grimes, Iowa 50111
- GENERAL DESCRIPTION:** This request would create 5 lots along Iowa Highway 28 that are proposed to be zoned C-3 as part of the Echo Valley Community PUD amendment request. The request would also create a large outlot to the east of Marketplace Drive for future development.
- IMPACT ON NEIGHBORHOOD:** Single family homes are to the north of the proposed development. The majority of the single family homes are adjacent to Outlot Y. Three lots are adjacent to the proposed commercial lot 1. This lot would require a buffer wall per the proposed Echo Valley Community PUD amendment. To the west across Iowa Highway 28 are single family homes and the New Life Lutheran Church.
- VEHICULAR & PEDESTRIAN TRAFFIC:** The plat shows the construction of a new street, Marketplace Drive. Marketplace Drive maintains the current intersection with Iowa Highway 28. The City is currently working with the Iowa DOT on a warrant study for a traffic signal at the intersection. There is a new intersection with Beardsley Street that is approximately 350' from Iowa Highway 28. Removing the Masteller intersection with Beardsley and relocating further to the east should improve traffic operation through the area. The street is 28' wide on the plat with it widening to 37' at the intersection of Beardsley Street. The proposed PUD amendment sets the street widths. Statewide Urban Design and Standards would typically require a 31' wide street in a commercial area.

TRAIL PLAN:	An 8' wide trail is shown on the east side of Marketplace Drive. A 10' wide trail easement is shown along Beardsley Street. The trail along Beardsley Street would eventually connect back to the east.
ZONING HISTORY FOR SITE AND IMMEDIATE VICINITY:	A portion of the site is zoned as Parcel J of the Echo Valley Community PUD in 2003 (Ordinance 03-08). The frontage along Iowa Highway 28 is zoned C-O. There is currently a PUD amendment being considered to make the entire site part of Parcel J of the Echo Valley Community PUD with a mix of C-3, R-2, R-3, and R-4, while restricting the R-4 uses to senior housing and assisted living.
BUFFERS REQUIRED/ NEEDED:	The proposed Echo Valley Community PUD amendment would require any C-3 lots along the northern boundary of the site to have a buffer wall. Buffering of uses interior to the site may be achieved via landscaping with no distance requirement to prompt a cohesive, mixed use development.
DRAINAGE:	<p>Drainage for the commercial lots is identified in two detention areas located on Outlot Y. Drainage is collected in a storm sewer system and discharged overland to the detention areas. There is no concern on the overland flow because the project is a single owner and Outlot Y will require further platting to be developed. At that time the overland flow will need to be addressed, either through the creation of easements or the development of an additional storm sewer system.</p> <p>Details of the design of the storm sewer system will be reviewed with the Construction Plans to ensure that detention areas are sized correctly.</p>
DEVELOPMENT HISTORY:	The area was planned as a PUD on July 14, 2004 and amended on June 4, 2015.
FLOODPLAIN:	None of the proposed lots are located within a floodplain.
PARKLAND:	No parkland dedication is required for the platting of commercial lots.
UTILITIES: WATER, SANITARY SEWER, STORM SEWER.	<ul style="list-style-type: none"> • An 8' water main is provided on the east side of Marketplace Drive. • Hydrants are shown along Marketplace Drive, additional hydrants will be needed on lots as they develop to ensure adequate lot coverage. • Sanitary sewer on the north end of the development runs in a 8' sewer on the west side of Marketplace Drive, servicing lots 1-3. The sewer connects across outlot Y to an existing sanitary sewer main on the east side of outlot Y. • On the south side of the development, an 8' sewer is along the west side of Marketplace Drive, serving lots 4 and 5. This sewer continues along the north side of Beardsley Street and connects to existing sanitary sewer to the east. • Several 15' storm sewers are throughout the site that collect drainage from lots 1-5 and the street. This systems outlets onto outlot Y and flows overland to the detention areas.

**RELATIONSHIP TO
COMPREHENSIVE LAND
USE PLAN:**

The future land use plan the majority of this area as General Commercial with a portion shown as Park/Recreation near the Golf Course. The plat will create commercial lots that are in accordance with the future land use plan.

**STAFF ANALYSIS –
ZONING ORDINANCE:**

The Preliminary Plat consists of 5 commercial lots and 1 outlot for future development. The plat consists of 27.85 acres of land east of Iowa Highway 28 and north of Beardsley Street. The commercial lots vary in size measuring from 37,044 SF to 107,296 SF. Outlot Y is 643,480 SF of future development ground and will require further platting.

Streets shown will be dedicated to the City for street use upon approval of the Final Plat. The designated street right-of-way is 60 feet with a 28' wide road. At the intersection of Marketplace Drive and Beardsley Street, the right-of-way widens to 65' with a 37' wide road to allow for turn lanes.

The area is currently being considered for a rezoning amendment to the Echo Valley Community PUD. The proposed PUD amendment would be for any commercial lots to be in the C-3 district with the following bulk regulations:

- Minimum lot area – 20,000 SF
- Minimum lot width – 100'
- Front Setback – 30'
- Side setback – 10' and 20' total
- Rear setback – 10'
- Height – 50'

**STAFF ANALYSIS –
SUBDIVISION
ORDINANCE:**

The proposed PUD amendment also allows for R-2, R-3, and R-4 type uses. These would potentially be developed in outlot Y. The Subdivision Ordinance requires that Preliminary Plat submissions details on lot design, street layout, sanitary sewer layout, water main layout, grading, and storm water management. All information has been submitted by the applicant.

**PLANNING AND ZONING
RECOMMENDATION:**

The Planning and Zoning Commission recommends that the request for the Preliminary Plat of Marketplace at Echo Valley be approved for the following conditions:

- That the details of the amendment to the Echo Valley Community PUD be incorporated into the Preliminary Plat.
- That the applicant provides all supporting documentation required within the Norwalk Subdivision Regulations.
- That any significant modifications to the plat be reviewed and approved by the Planning & Zoning Commission and City Council.

ATTACHMENTS:

Attachment "A" – Marketplace at Echo Valley Preliminary Plat
Attachment "B" – Marketplace at Echo Valley Vicinity Map

<input checked="" type="checkbox"/> Resolution _____ Ordinance _____ Contract _____ Other (Specify) _____
Funding Source: _____ NA
<p>APPROVED FOR SUBMITTAL:</p> <p style="text-align: center;"> Planning & Economic Development Director</p>

RESOLUTION NO. ____

A RESOLUTION APPROVING THE MARKETPLACE AT ECHO VALLEY PRELIMINARY PLAT

WHEREAS, the Planning & Zoning Commission reviewed this request at their regular meeting on March 28, 2016 and recommends approval of the Preliminary Plat; and

WHEREAS, that the details of the amendment to the Echo Valley Community PUD be incorporated into the Preliminary Plat; and

WHEREAS, that the applicant provides all supporting documentation required within the Norwalk Subdivision Regulations; and

WHEREAS, that any significant modifications to the plat be reviewed and approved by the Planning & Zoning Commission and City Council; and

NOW, THEREFORE, BE IT RESOLVED: That the City Council does hereby approve the Preliminary Plat for the Marketplace at Echo Valley as described and shown in Attachment "A" attached hereto and made a part thereof by reference.

PASSED AND APPROVED this 5th day of May, 2016.

Tom Phillips - Mayor

ATTEST:

JODI EDDLEMAN, CITY CLERK

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Kuhl	___	___
Lester	___	___
Isley	___	___
Riva	___	___
Livingston	___	___

MARKETPLACE AT ECHO VALLEY

PRELIMINARY PLAT (SHEET 1 OF 3)

OWNER / DEVELOPER

UNITED PROPERTIES INVEST CO LC
C/O MICHAEL COPPOLA
4521 FLEUR DRIVE, SUITE C
DES MOINES, IOWA 50321

ENGINEER / SURVEYOR

CIVIL DESIGN ADVANTAGE, LLC
3405 S.E. CROSSROADS DRIVE, SUITE G
GRIMES, IOWA 50111
PH: 515-369-4400

BENCHMARK

BM#1
BURY BOLT ON HYDRANT NE CORNER OF BEARDSLEY STREET AND MASTELLER ROAD. ELEVATION=895.14

BM#2
BURY BOLT HYDRANT ON WEST SIDE OF ECHO RIDGE TRAIL 500'+- NORTH OF BEARDSLEY STREET. ELEVATION=936.02

ZONING

ECHO VALLEY COMMUNITY PLANNED UNIT DEVELOPMENT
PARCEL J - MIX OF C-3, R-4, R-3 & R-2

BULK REGULATIONS

MIN LOT AREA:
C-3 = 20,000 SF
R-4 = 80,000 SF & 1,250 SF/UNIT
R-3 = 3,125 SF/UNIT OR POSTAGE STAMP LOTS 1,250 SF/UNIT

R-2 TWO-FAMILY = 12,500 SF
R-2 ONE-FAMILY = 8,125 SF

MIN LOT WIDTH:
C-3 = 100'
R-4 = 200' PROJECT
R-3 = 200' PROJECT & 20' INDIVIDUAL UNIT
R-2 TWO-FAMILY = 40' WITH 0' SIDE YARD OR 80' FOR TWO UNITS ON ONE LOT
R-2 ONE-FAMILY = 65'

FRONT SETBACK:
C-3 = 30'
R-4 = 35'
R-3 = 30' PROJECT OR 25' TO CURB FOR PRIVATE STREETS ***
R-2 TWO-FAMILY = 25' ***
R-2 ONE-FAMILY = 25' ***

SIDE SETBACK (EACH SIDE):
C-3 = 10'
R-4 = 20' PROJECT *
R-3 = 0' SHARED WALL & 12' BUILDING SEPARATION
R-2 TWO-FAMILY = 5' OR 0' SHARED WALL
R-2 ONE-FAMILY = 5'

SIDE SETBACK (TOTAL):
C-3 = 20'
R-4 = N/A
R-3 = N/A
R-2 TWO-FAMILY = 10'
R-2 ONE-FAMILY = 10'

REAR SETBACK:
C-3 = 10'
R-4 = 35'
R-3 = 30' PROJECT OR 20' FOR LOTS INTERNAL TO THE PROJECT
R-2 TWO-FAMILY = 30'
R-2 ONE-FAMILY = 30'

HEIGHT:
C-3 = 50'
R-4 = 45' **
R-3 = 35'
R-2 TWO-FAMILY = 35'
R-2 ONE-FAMILY = 35'

* 20' MINIMUM SEPARATION BETWEEN BUILDINGS IN A COMPLEX.
** PRINCIPAL STRUCTURE MAY BE 45' IN HEIGHT BUT NOT EXCEED 3 FLOORS ABOVE GRADE.
*** COVERED FRONT PORCHES THAT ARE FULLY OPEN AND NOT ENCLOSED MAY ENCRUCH EIGHT (8) FEET INTO THE FRONT YARD SETBACK.

NOTES

- NO LOTS SHALL HAVE DIRECT ACCESS TO HIGHWAY 28 OR BEARDSLEY STREET.
- ANY USE OF A PUBLIC UTILITY EASEMENT BY A PARTY OTHER THAN THE CITY OF NORWALK IS SUBORDINATE TO THE CITY'S USE OF THE EASEMENT FOR A DESIGNATED CITY UTILITY PURPOSE. ANY PARTY OTHER THAN THE CITY USING THE PUBLIC UTILITY EASEMENT MUST RELOCATE THEIR FACILITIES AT NO COST TO THE CITY TO ACCOMMODATE THE CITY'S USE OF ITS DESIGNATED UTILITY.
- STREET LOTS A & B TO BE DEDICATED TO THE CITY OF NORWALK FOR RIGHT-OF-WAY PURPOSES.

PRELIMINARY PLAT DESCRIPTION

A PART OF THE SOUTH 2 ACRES OF THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER AND A PART OF THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER, ALL IN SECTION 6, TOWNSHIP 77 NORTH, RANGE 24 WEST OF THE FIFTH PRINCIPAL MERIDIAN IN THE CITY OF NORWALK, WARREN COUNTY, IOWA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID SECTION 6; THENCE NORTH 86°27'27" EAST ALONG THE SOUTH LINE OF SAID SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER, 49.70 FEET TO THE SOUTHWEST CORNER OF AN EXISTING ROADWAY EASEMENT RECORDED IN BOOK 218, PAGES 85-86 AND THE POINT OF BEGINNING; THENCE NORTH 01°11'04" WEST ALONG THE WESTERLY LINE OF SAID ROADWAY EASEMENT, 27.20 FEET; THENCE NORTH 86°30'01" EAST CONTINUING ALONG SAID WESTERLY LINE, 20.00 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF IOWA HIGHWAY NO. 28; THENCE NORTH 46°50'31" WEST ALONG SAID EAST RIGHT-OF-WAY LINE, 27.45 FEET; THENCE NORTH 01°11'04" WEST CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE, 475.88 FEET; THENCE NORTH 00°55'22" WEST CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE, 232.00 FEET; THENCE NORTH 07°07'01" EAST CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE, 435.51 FEET; THENCE NORTH 02°27'19" EAST CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE, 80.00 FEET; THENCE NORTH 00°55'45" EAST CONTINUING ALONG SAID EAST RIGHT-OF-WAY LINE, 121.40 FEET TO A POINT ON THE NORTH LINE OF SAID SOUTH 2 ACRES OF THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER, SAID POINT BEING THE SOUTHWEST CORNER OF LOT 1, ECHO VALLEY ESTATES, AN OFFICIAL PLAT IN SAID CITY OF NORWALK; THENCE NORTH 87°39'43" EAST ALONG SAID NORTH LINE AND THE SOUTH LINE OF SAID ECHO VALLEY ESTATES AND THE EASTERLY EXTENSION THEREOF, 1146.08 FEET TO THE WESTERLY LINE OF PARCEL 'C' AS SHOWN ON THE PLAT OF SURVEY RECORDED IN IRREGULAR PLAT BOOK 19, PAGE 6 OF 77-24; THENCE SOUTH 9°46'10" WEST ALONG SAID WESTERLY LINE, 98.72 FEET; THENCE SOUTH 25°15'42" WEST CONTINUING ALONG SAID WESTERLY LINE, 601.62 FEET; THENCE SOUTH 17°45'32" WEST CONTINUING ALONG SAID WESTERLY LINE, 654.41 FEET; THENCE SOUTH 3°08'26" EAST CONTINUING ALONG SAID WESTERLY LINE, 132.09 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL 'C'; THENCE SOUTH 86°27'27" WEST ALONG THE SOUTH LINE OF SAID SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER, 680.21 FEET TO THE POINT OF BEGINNING AND CONTAINING 27.85 ACRES (1,213,358 SQUARE FEET).

PROPERTY IS SUBJECT TO ANY AND ALL EASEMENTS OF RECORD.

VICINITY MAP



NORWALK, IOWA

SHEET INDEX

SHEET 1: PRELIMINARY PLAT (DIMENSION PLAN)
SHEET 2: PRELIMINARY PLAT (GRADING PLAN)
SHEET 3: PRELIMINARY PLAT (UTILITY PLAN)

GENERAL LEGEND

PROPOSED FEATURES

- TYPE SW-501 STORM INTAKE
- TYPE SW-503 STORM INTAKE
- TYPE SW-505 STORM INTAKE
- TYPE SW-506 STORM INTAKE
- TYPE SW-513 STORM INTAKE
- TYPE SW-401 STORM MANHOLE
- TYPE SW-402 STORM MANHOLE
- TYPE SW-301 SANITARY MANHOLE
- STORM/SANITARY CLEANOUT
- WATER VALVE
- FIRE HYDRANT ASSEMBLY
- SIGN
- DETECTABLE WARNING PANEL
- SANITARY SEWER WITH SIZE
- SANITARY SERVICE
- STORM SEWER
- STORM SERVICE
- WATERMAIN WITH SIZE
- WATER SERVICE
- SAWCUT (FULL DEPTH)
- SILT FENCE

EXISTING FEATURES

- SANITARY MANHOLE
- WATER VALVE BOX
- FIRE HYDRANT
- WATER CURB STOP
- WELL
- STORM SEWER MANHOLE
- STORM SEWER SINGLE INTAKE
- STORM SEWER DOUBLE INTAKE
- FLARED END SECTION
- DECIDUOUS TREE
- CONIFEROUS TREE
- DECIDUOUS SHRUB
- CONIFEROUS SHRUB
- ELECTRIC POWER POLE
- GUY ANCHOR
- STREET LIGHT
- POWER POLE W/ TRANSFORMER
- UTILITY POLE W/ LIGHT
- ELECTRIC BOX
- ELECTRIC TRANSFORMER
- ELECTRIC MANHOLE OR VAULT
- TRAFFIC SIGN
- TELEPHONE JUNCTION BOX
- TELEPHONE MANHOLE/VAULT
- TELEPHONE POLE
- GAS VALVE BOX
- CABLE TV JUNCTION BOX
- CABLE TV MANHOLE/VAULT
- MAIL BOX
- BENCHMARK
- SOIL BORING
- UNDERGROUND TV CABLE
- GAS MAIN
- FIBER OPTIC
- UNDERGROUND TELEPHONE
- OVERHEAD ELECTRIC
- UNDERGROUND ELECTRIC
- FIELD TILE
- SANITARY SEWER W/ SIZE
- STORM SEWER W/ SIZE
- WATER MAIN W/ SIZE

SURVEY

- SECTION CORNER
- 1/2" REBAR, YELLOW CAP #18660 (UNLESS OTHERWISE NOTED)
- ROW MARKER
- ROW RAIL
- PLATTED DISTANCE
- MEASURED BEARING & DISTANCE
- RECORDED AS
- DEED DISTANCE
- CALCULATED DISTANCE
- CURVE ARC LENGTH
- MINIMUM 100 YEAR FLOOD PROTECTION ELEVATION
- CENTERLINE
- SECTION LINE
- 1/4 SECTION LINE
- 1/4 1/4 SECTION LINE
- EASEMENT LINE
- LOT LINE
- PLAT BOUNDARY

- FOUND
- SET
- SECTION CORNER
- ROW MARKER
- ROW RAIL
- PLATTED DISTANCE
- MEASURED BEARING & DISTANCE
- RECORDED AS
- DEED DISTANCE
- CALCULATED DISTANCE
- CURVE ARC LENGTH
- MINIMUM 100 YEAR FLOOD PROTECTION ELEVATION
- CENTERLINE
- SECTION LINE
- 1/4 SECTION LINE
- 1/4 1/4 SECTION LINE
- EASEMENT LINE
- LOT LINE
- PLAT BOUNDARY

18660

PRELIMINARY

NOT FOR CONSTRUCTION

I HEREBY CERTIFY THAT THIS LAND SURVEYING DOCUMENT WAS PREPARED AND THE RELATED SURVEY WORK WAS PERFORMED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL LAND SURVEYOR UNDER THE LAWS OF THE STATE OF IOWA.

DATE

THIS SHEET

DATE: 03/22/16

REVISIONS:

SECOND SUBMITTAL

FIRST SUBMITTAL

3405 S.E. CROSSROADS DRIVE, SUITE G
GRIMES, IOWA 50111
PHONE: (515) 369-4400 FAX: (515) 369-4410

TECH: LMK

ENGINEER: RDR

MARKETPLACE AT ECHO VALLEY

PRELIMINARY PLAT (DIMENSION PLAN)

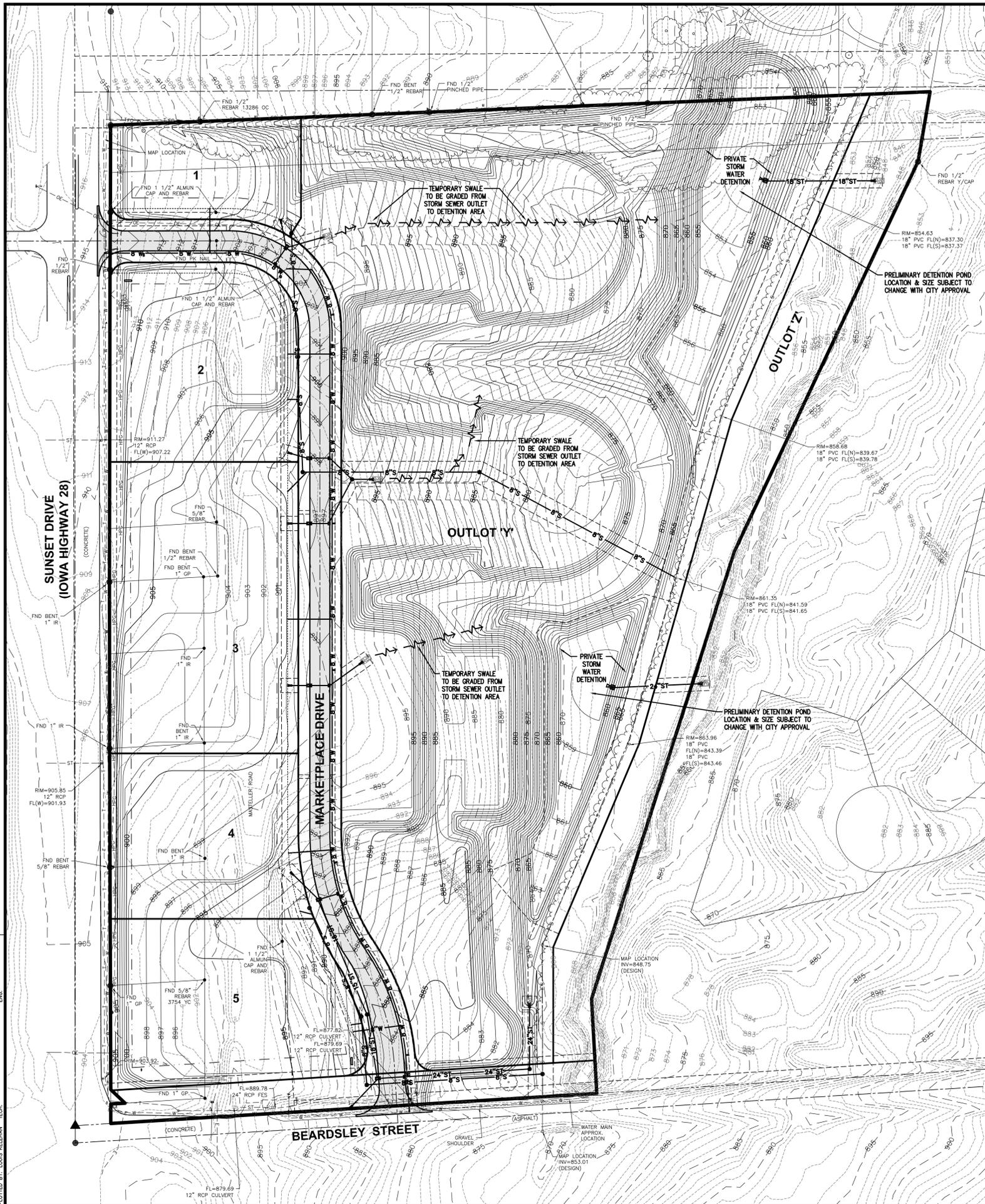
NORWALK, IOWA

1 / 3

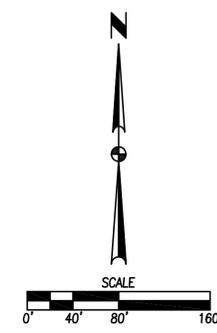
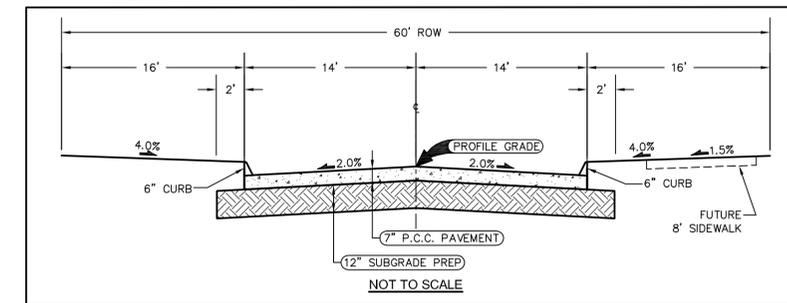
1507.367

MARKETPLACE AT ECHO VALLEY

PRELIMINARY PLAT (SHEET 2 OF 3)



TYPICAL 28' B/B STREET SECTION (60' ROW)



FILE: H:\1507367\1507367.DWG - PRELIMINARY - PLAT.DWG
 PLOTTED BY: (02/27/2016 2:45 PM)
 COMMENT: (02/27/2016 2:45 PM)
 DRAWN BY: (02/27/2016 2:45 PM)

REVISIONS	DATE
	03/22/16
	02/17/16

3405 S.E. CROSSROADS DRIVE, SUITE G
 GRIMES, IOWA 50111
 PHONE: (515) 369-4400 FAX: (515) 369-4410



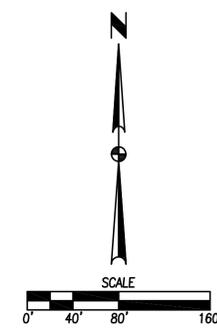
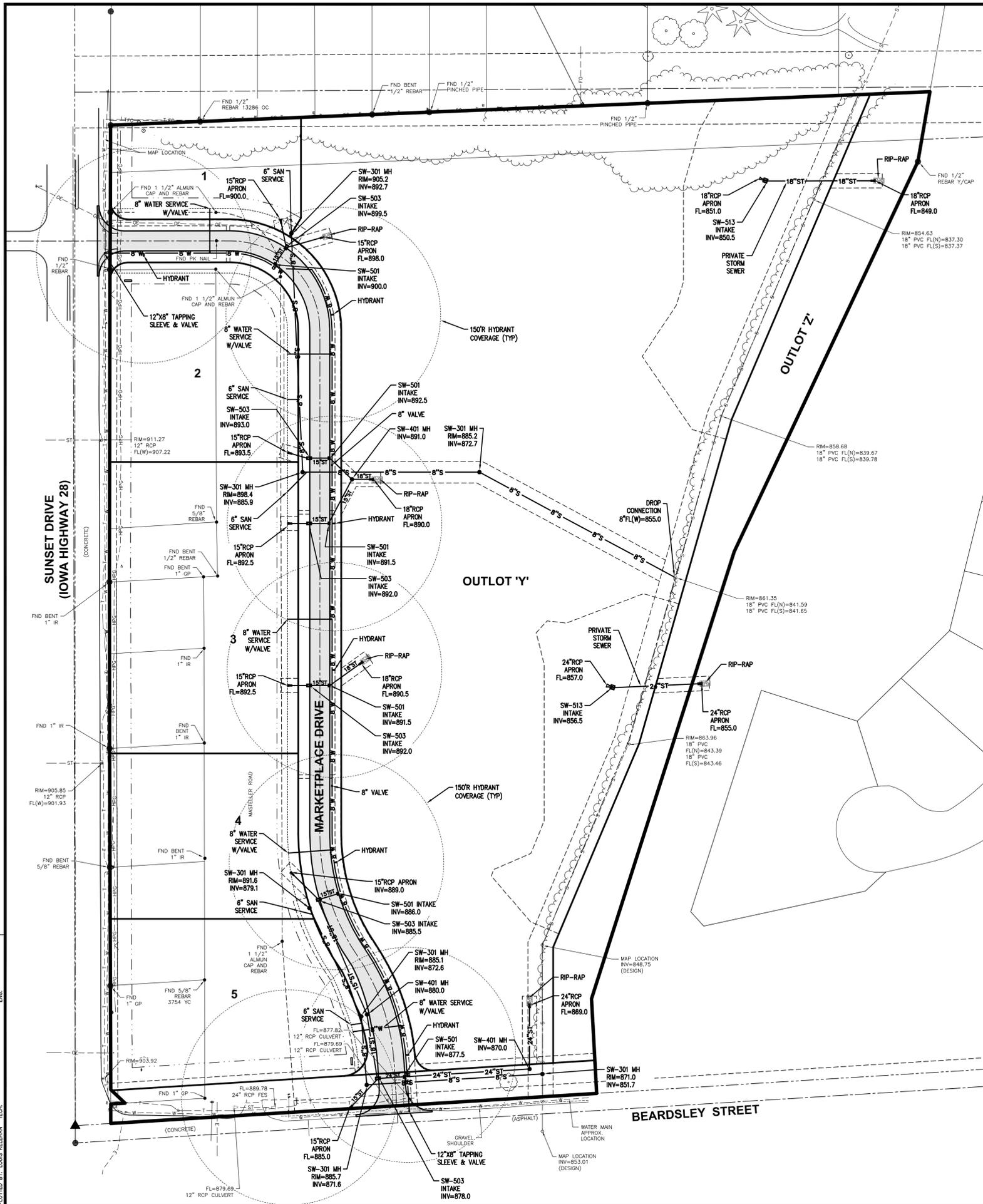
MARKETPLACE AT ECHO VALLEY
 PRELIMINARY PLAT (GRADING PLAN)
 NORWALK, IOWA

SECOND SUBMITTAL
 FIRST SUBMITTAL

ENGINEER: RDR
 TECH: LMK

MARKETPLACE AT ECHO VALLEY

PRELIMINARY PLAT (SHEET 3 OF 3)



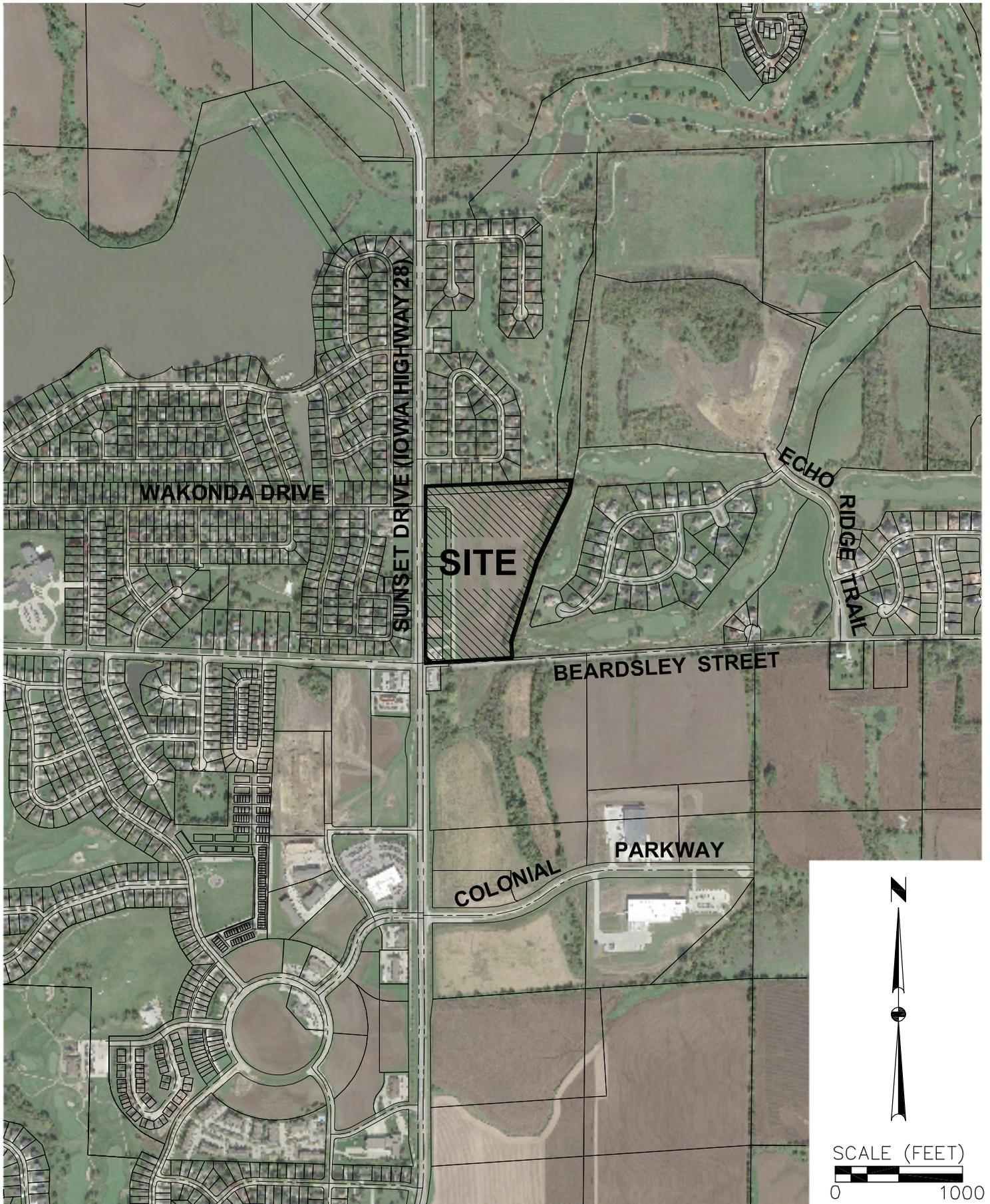
REVISIONS	DATE
	03/22/16
	02/17/16

3405 S.E. CROSSROADS DRIVE, SUITE G
 GRIMES, IOWA 50111
 PHONE: (515) 369-4400 FAX: (515) 369-4410
 ENGINEER: RDR TECH: LMK



MARKETPLACE AT ECHO VALLEY
PRELIMINARY PLAT (UTILITY PLAN)
 NORWALK, IOWA

FILE: H:\1507367\1507367-PLAT.DWG
 COMMENT: PRELIMINARY-PLAT.DWG
 PLOTTED BY: LOUIS WELSHAN
 DATE: 3/22/2016 2:46 PM



SCALE (FEET)





**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 15
For Meeting of 05/05/2016

ITEM TITLE: Discussion and possible action regarding request for urban chickens.

CONTACT PERSON: Luke Parris

SUMMARY EXPLANATION: Resident, Stacey Squiers inquired about the Municipal Code for the City of Norwalk regarding urban poultry. Staff indicated that there is not currently an ordinance in place for keeping egg laying chickens in town and that she could go before council to request permission. She then presented a petition signed by her neighbors agreeing to allow her to have the chickens.

The council has considered criteria for urban chickens in the past however nothing was adopted. The city code addresses chickens in chapter 55.05 as livestock. Chapter 55.05 states that it is unlawful for a person to keep livestock within the City except by written consent of the Council or except in compliance with the City's zoning regulations.

The zoning requirement states that they have to meet setback requirements for an accessory structure. 5 feet from the side lot line, 5 feet from the rear lot line and 10 feet from any other structure. An accessory structure permit would be required for the chicken coop.

If the council would like to consider how the city addresses urban chickens, city staff could develop a proposed ordinance or amendment to the zoning ordinance to more formally address the issue.

STAFF RECOMMENDATION: Consider and advise

I approve of my neighbor, Stacey Squiers, having 4 egg layers in her back yard and know she will not have any roosters in this small flock.

1313 Main St.

Neighbor's Printed Name & Signature	Neighbor's Address
Jim Becker Jim Becker	1318 Main St
Tara Badger Tara Badger	1405 Main Street
Joe Hill Joe Hill	1325 Main.
Jean Stansberry	1324 Main St
TONY BALDWIN	1314 MAIN ST
Rick Ward	1305 Main St.
Patty Saunders	1221 Main St.
Tanya McAninch Tanya McAninch	1213 Main St Norwalk, IA 50211

Sue Coffey

1302 Main St.
Norwalk, IA 50211


BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT

Item No. 16
For Meeting of 5.5.2016

ITEM TITLES: Consider request by Herb Eckhouse of LaQuercia for the City to co-sponsor as an economic development business retention event, their front yard party during the American Cheese Festival on July 26, 2016.

CONTACT PERSONS: Wade R. Wagoner, AICP LEED GA, Planning and Economic Development Director

SUMMARY EXPLANATION The City of Norwalk staff received a request from Herb Eckhouse of LaQuercia to ask if the City would support his request to use School District school buses for a lawn party he is having at his site in conjunction with the American Cheese Festival on July 26, 2016.

The school district needs the City to cosponsor the event to establish a "public purpose" for the use of the buses.

Attached you will find interaction from Mr. Eckhouse, and from Kate Baldwin representing the school district.

Staff asks that the Council consider a simple motion on the request.

Potential Benefits A part of any good economic development strategy is a strong business retention plan. The American Cheese Festival is a once in a generation event that allows us to showcase Norwalk.

Other Considerations: The potentially precedent setting action places the council in the position of evaluating private business' request for school buses on a case by case basis and determining if each request rises to a business retention level criteria. The city has not developed any parameters to assist in making that determination.

Final Decision: If the City favorably recommends, the final decision rests with the school board.

____Resolution ____ Ordinance ____ Contract ____Other (Specify)____ <u>X</u> ____
Funding Source: <u>Approve Request by simple motion</u>

APPROVED FOR SUBMITTAL _____ Planning & Economic Development Director City Attorney

STAFF RECOMMENDATION: Consider potentially precedent setting action and approve or deny by simple motion.

Written by Herb Eckhouse – La Quercia

La Quercia Front Yard Party in Norwalk—July 26, 2016

Why: The American Cheese Society (ACS), the leader in promoting and supporting the flourishing American cheese industry, will hold their annual conference in Des Moines. This is a gathering of the most impassioned food enthusiasts in the country, most of whom buy and resell La Quercia cured meats, made in Norwalk. This is a unique, once in a generation opportunity to bring them to see how we make the meats that they enthusiastically present to their customers in every state in the Country.

What: We want to provide as many conference participants as possible the opportunity to tour our Norwalk facility, so we are hosting a Front Yard Party—transporting guests from Des Moines to Norwalk, offering them pizza made on site, sweet corn, fresh Iowa tomatoes, beer and soft drinks and offering a tour of our facility. Seeing our building and our operation is the greatest way for us to create customer loyalty and to advance the success of our Norwalk based business.

Who: This event will be cohosted by La Quercia, based in Norwalk, and Hy-Vee. We invited the City of Norwalk to join as a host.

When: The event will be from 6-9 PM, July 26, 2016. Though the Conference starts July 28, hundreds of attendees will arrive July 25-26 to judge the cheese competition and/or study and take exams for Cheese Professional accreditations. The ACS will help us inform attendees of this event.

Request: We believe that we could greatly enhance the hometown, Iowa feel of this event, and support the image of the Norwalk community by having all guests picked up in Des Moines by busses from the Norwalk Community School District. This will give immediate visibility to Norwalk and create an informal, welcoming feel for all guests from the City. Therefore we request that, to promote and enhance the image of the City of Norwalk, the school district provide school busses (compensated for relevant expenses) to transport guests from Des Moines to Norwalk the evening of July 26, 2016 from 5-10 PM.

Written by Kate Baldwin:

Wade,

RE: Herb Eckhouse, La Quercia request for the School to provide transportation services

I know you had difficulty hearing all of the conversation during our conference call this am. I decided to follow up with you, so you understand the limitations of the District to offer school buses for non-school, non- student related purposes and options that we have available to meet Herb Eckhouse's request.

As I understand, Herb is wanting to hire the School to provide buses to transport convention guests from the Iowa Events Center to La Quiera for a private open house sponsored by La Quercia. These guests would be members of a national association, called the American Cheese Society. La Quercia has a vested interest in promoting their business by hosting the association's membership to an open house. Only convention members will be invited to attend the La Quercia event in Norwalk. Herb is estimating that about 400 guests out of the 2400 convention attendees would elect to come out to visit the La Quercia plant. He will be hosting the event from 6:00 pm to 9 pm in lawn-style party tent, serving pizza, beer, sweet corn.

We do not have a policy that openly forbids the School District from providing transportation, but the Iowa DE is clear that any use of local tax-supported school buses must meet a public purpose. On face value, I am struggling to find any public

purpose for the School to provide transportation to this event for the following reasons:

1. It is not, nor could be a school sponsored event
2. It does not involve transporting any students, specially Norwalk students
3. It is not a local community event/festival, involving Norwalk citizens
4. It is a private, business type event.
5. It is not open to the general public

Now with that being said, the criteria above is not that much different than when the School District provided shuttle transportation for the Greater Des Moines Home Show in 2013. In addition to most of the criteria above, people actually paid to attend the Home Show. However, the School District was able to define "public purpose" to provide transportation for the Home Show because the City agreed to sponsor the Home Show as local economic development. The City determined the event promoted the City, School and community of Norwalk, and agreed to reimburse the School for all actual costs incurred. Herb has agreed to pay for the transportation costs. That is not an issue. But in order for the School to consider this transportation request, the City of Norwalk will need to sponsor/endorse the event as a local economic development event. It was a partnership effort between the School District and the City. When I consider the public limitations of using school buses, that will be the only way the School District will be able to meet Herb's request.

I know that Herb was disappointed and maybe a little frustrated that I couldn't agree on the spot to provide transportation. I

talked with Denny and we want to cooperate with local Chamber/businesses while maintaining integrity to the accountability of public purpose. The only way I see it this happening is really through a City sponsorship.

I wanted to get this information down in an email to you so the City could consider the District's position along with how the City might respond. I am welcome to visit more with you, Tom Phillips or other City officials about how the District can cooperate if the City is willing to support this event.

Take care,

Kate

From: Kate Baldwin [<mailto:kbaldwin@norwalk.k12.ia.us>]

Sent: Wednesday, April 27, 2016 3:28 PM

To: Wade Wagoner

Cc: Herb Eckhouse; Mayor; Rick Kaul; Denny Wulf

Subject: Re: Agenda item on May 5

Hi Herb, Wade and Tom,

I think we are making great progress to approve La Quercia's request for the School to provide bus transportation for the July 26th National Cheese Society local event. I have shared the details of this promotional event with Rick Kaul, our school board president. I also shared the City's proposed commitment to sponsor the event. Rick Kaul will recommend, with Denny and me, that the School provide the transportation as a partnership activity with the City. We all know how valuable our local businesses are to the School and the City. Through a cooperative effort with the City we were able to make this happen within our scope of legal responsibilities.

I am planning to attend the May 5th City Council meeting to express the School's support of the event. If approved by the Council, I will add this item for final approval by the School Board at next meeting, which is tentatively scheduled for May 23rd.

Good luck planning your event.

Kate



**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 17
For Meeting of 5.5.2016

ITEM TITLES: Consideration of the resolution approving the an addendum to the Development Agreement with Farms of Holland LLC dated 7/16/2015

CONTACT PERSONS: Wade R. Wagoner, AICP LEED GA, Planning and Economic Development Director
Jim Dougherty, City Attorney

SUMMARY EXPLANATION

The City of Norwalk has been working with the Farms of Holland on a Development Agreement Addendum.

If requested by Council at the meeting, staff is ready with a power point presentation that outlines staff decision making process.

Attached is a development agreement addendum reviewed by both the Holland Family attorney, Bob Stuyvesant, and City Attorney, Jim Daugherty.

Benefits of New Development Agreement and park redesign.

- Eliminates expensive excess soils export costs.
- Improves the park and creates a trail head/parking lot
- Improves value of Commercial pad sites for seller and for future tax revenues
- Expedites commercial development in Norwalk

Resolution Ordinance Contract Other (Specify) _____

Funding Source: TIF



APPROVED FOR SUBMITTAL _____ Planning & Economic Development Director
Jim Dougherty City Attorney

STAFF RECOMMENDATION: Approve resolution on a roll call vote.

RESOLUTION NO. _____

Approving the Development Agreement Addendum with Farms of Holland LLC

WHEREAS, City will acquire additional ground to accommodate redesign of the park, including a trail head/parking; and

WHEREAS, the new design of the park "balances" excess soils through a private/public partnership that eliminates the need for the costly soils export; and

WHEREAS, pad sites are improved, adding value to both the City and the seller; and

WHEREAS, because the costly export has been eliminated, city agrees as to include as a part of the contract (the cheapest time to do it) a provision to spread and compact the excess soils (a provision that was already contemplated for the road); and

WHEREAS, seller agrees to not reasonable reject the excess soils if they are compactable;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF NORWLAK, IOWA, that the attached development agreement with Farms of Holland, attached hereto and made a part hereof, is approved.

Passed and approved this 5th day of May, 2016.

Tom Phillips, Mayor

ATTEST:

Jodi Eddleman, City Clerk

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Riva	___	___
Isley	___	___
Kuhl	___	___
Livingston	___	___
Lester	___	___

AMENDED DEVELOPMENT AGREEMENT

THIS AMENDMENT is made and entered into on the day of May, 2016, by and between the City of Norwalk, Iowa (hereinafter referred to as Norwalk) and Farms of Holland LLC (“FOH”):

RECITALS

WHEREAS, on or about July 16, 2015 Farms of Holland LLC and Norwalk entered into a Development Agreement involving, in part, the City constructing a regional storm water detention facility (“Facility”) upon ground to be donated by FOH; and

WHEREAS, at the time of entering into the Development Agreement, only preliminary plans for the Facility had been created. Additional analysis and planning relating to the Facility has now been completed. As a result of the additional analysis and planning, Norwalk and FOH find it necessary and mutually beneficial to enter into this Amended Agreement.;

NOW, THEREFORE, the parties hereto agree the Development Agreement as follows:

1. FOH agrees to donate to the City additional property, as described in Exhibit “A” attached hereto. The property shall be conveyed to the City via Warranty Deed. FOH shall provide the City with an Abstract of Title for the property being conveyed showing good and marketable title in the property, free of all liens or other encumbrances to the satisfaction of the City. The conveyance shall occur in conjunction with and within 45 days of the execution of this Agreement by the parties.
2. The additional property donated by FOH is needed by the City for the purpose of providing access to and parking for the Facility. The City intends to construct access and parking in a future phase of the Facility, the timing of which is not yet determined. Upon construction, the Owner of Outlot U in Farms of Holland Plat 2, an Official Plat, now included in and forming a part of the City of Norwalk, Warren County, Iowa shall be provided a non-exclusive Ingress/Egress easement over the Southern 120 feet of the access area providing access for automobile traffic from Colonial Parkway to the property. The easement shall be perpetual in nature and shall run with the land. The City reserves the right to place reasonable restriction on the easement area regarding commercial vehicle weights and uses. The future owner of Outlot U and the City will work out the details of joint responsibility, based on traffic loading, and for any additional modification, maintenance or expenses caused as a result of the use by both parties. It is understood that if the City is the first to develop the trail head. The entrance will, for the full length of the 120’ easement in addition to the ingress/egress in the r-o-w, be constructed to accommodate commercial truck traffic.
3. Paragraph 8 of the Development Agreement is hereby amended to state;

Prior to delivering the soils, the City will conduct on-site sampling and testing of the soils excavated from the Property during construction, with a frequency to be determined by the geotechnical engineer. The on-site sampling will be provided by a geotechnical engineer hired by the City. Such excavated soils will be observed and tested using the Standard Proctor Test (ASTM D698). The geotechnical engineer shall designate the excavated soils as containing structural properties or non-structural properties. This designation shall be evaluated with the Standard Proctor Test results along with the professional opinion of the geotechnical engineer. The City and owner of property shall mutually agree to the designation. Soils tested with a minimum dry density of 100 pounds per cubic foot (pcf) will be placed in areas designated for structural fill. Soils tested with a dry density less than 100 pcf will be placed in areas designated for non-structural fill. FOH reserves the right to reject soils tested with a dry density of less than 95 pcf. The results of the Standard Proctor as defined shall not be used as the sole designation. The geotechnical engineer shall evaluate all soil properties to formulate the designation. Upon delivery of such excavated soils, the City shall spread and compact the soils to the limits as specified in the Facility grading plan. The compaction of these soils shall be done per the recommendations of the geotechnical engineer based on the properties intended use. A Construction Manager hired by the City will be utilized to direct the placement of soil based on the information gathered by the geotechnical engineer.

The excess soils with the highest structural properties shall be directed to be placed and compacted by the Construction Manager in the following order of priority; the base for the new Turnberry Road to be constructed, the property owned by K F and H and City State Bank, Outlets U and V and new FOH City Park. The Construction Manager will make final determination on placement of soil if there is a difference of opinions.

Once the excess soils are delivered to adjacent properties, placed and compacted by the City, the owner of the property shall be responsible for ownership of all the delivered soils and use and regulations related thereto and shall hold the City harmless therefore. (ie erosion control, DNR regulations, requirements for additional geotechnical exploration for specific use, etc.)

The parties understand the risk associated with use of any such soils, structural or non-structural, delivered to the specified parcels by the City. Such risks may include but not be limited to settlement of fill areas over existing compressive soils and construction of movement sensitive structures, foundations, floor slabs and pavement over un-stabilized expansive soils.

4. The City, with FOH cooperation, shall obtain separate easements from the owners of adjacent properties, for no further consideration, for the placement of soils on adjacent properties.
5. All other terms of the Development Agreement described above shall remain in full force and affect.

The City and FOH have caused this Agreement to be signed, and the City's seal to be affixed, in their names and on their behalf by their duly authorized officers, all as of the day and date written above.

CITY:
CITY OF NORWALK, IOWA

By: _____
_____, Mayor

Attest:

_____, City Clerk
(Seal)

FOH:
Farms of Holland LLC

By: _____
James B Holland, Member/Manager

OWNER:
FARMS OF HOLLAND, LLC

EXHIBIT "A"

FOR PROPOSED ACQUISITION
FOR THE CITY OF NORWALK, IA

PARCEL ID:
64177024000

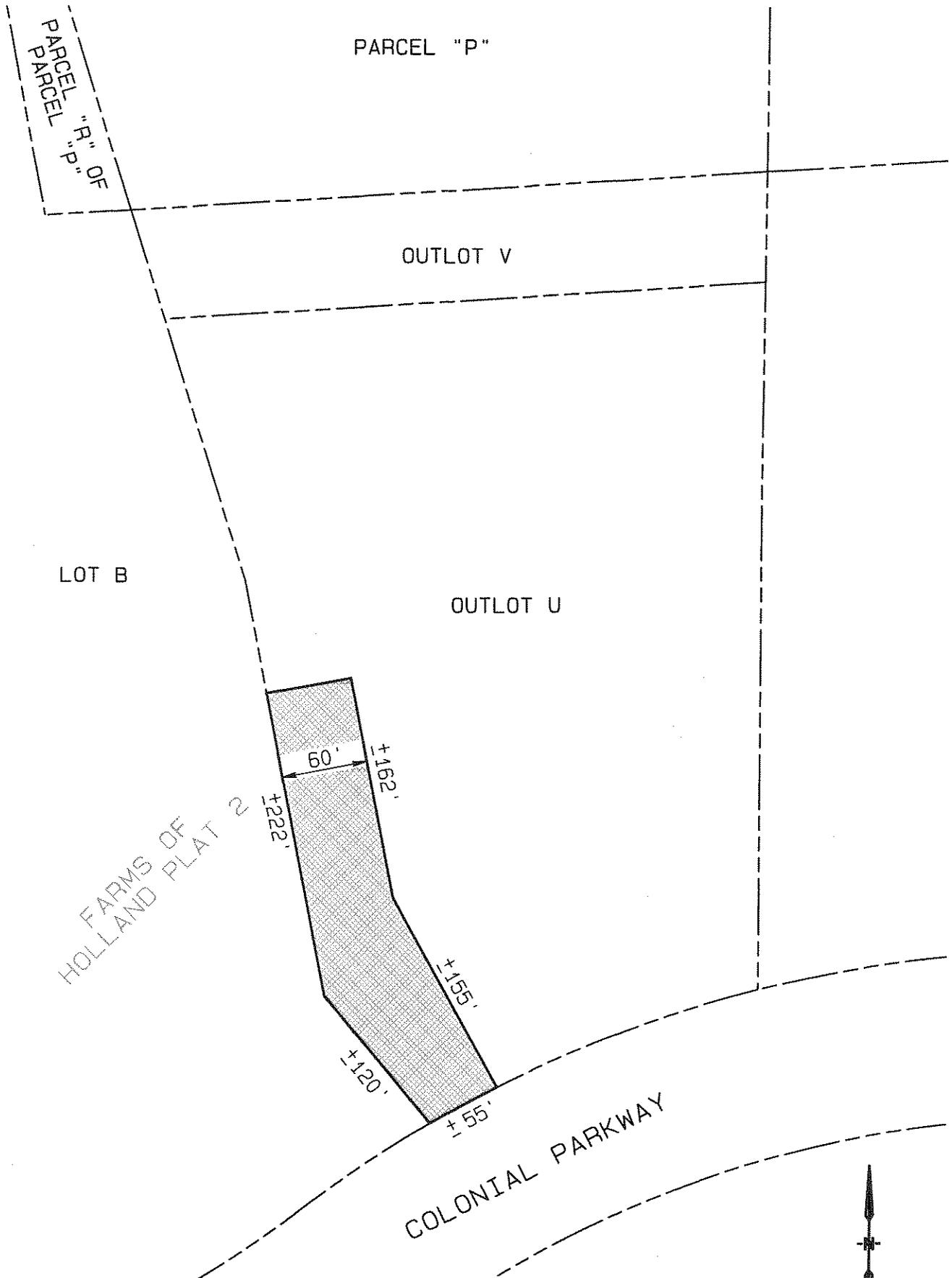
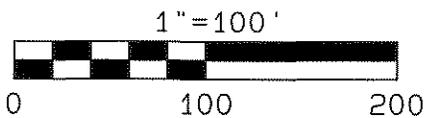


EXHIBIT AREA
21,171 SQUARE FEET
OR 0.49 ACRES



SCALE	1"=100'
DRAWN	TJP
CHECKED	BJM
APPROVED	AJB
DATE	04-22-15
A.C.	



EXHIBIT "A" IN
OUTLOT U, FARMS OF
HOLLAND PLAT 2

DWG. NO.

EX-1
1 OF 1

VEENSTRA & KIMM, INC. 3000 Westown Parkway • West Des Moines, Iowa 50266-1320
515-225-8000 • 515-225-7848 (FAX) • 800-241-8000 (WATS)

PROJECT 1427B



**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 18
For Meeting of 5.5.2016

ITEM TITLE: COLA salary comparisons

CONTACT PERSON(S): Jean Furler, Finance Director

SUMMARY EXPLANATION: The following cities were contacted regarding cost-of-living-adjustments (COLAS):

	Pop	COLA FY17	
Altoona	14,541	3.00%	
Ankeny	45,542	2.00%	
Carlisle	3,876	2.50%	
Clive	15,447	2.75%	
Grimes	8,246	3.00%	
Indianola	14,782	3.00%	
Johnston*	17,278		
Newton	15,254	3.25%	
Pleasant Hill	8,785	3.00%	
Urbandale	39,463	3.50%	
Waukee**	13,790		
West Des Moines	56,609	3.50%	
*CC yet to approve they have a 3% pool based on performance			
**Union contract negotiations underway (3% budgeted)			

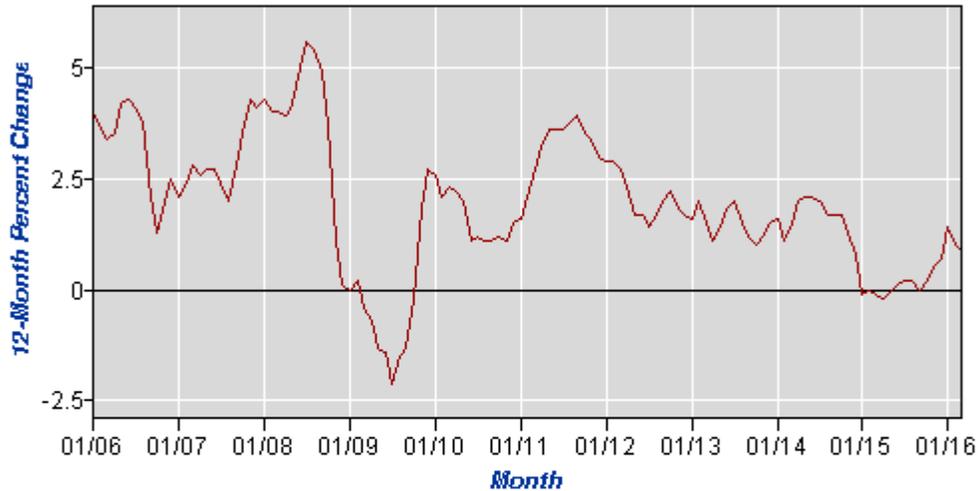
The above information was provided at the last council meeting. In addition, below is the information for the CPI trend:

Data extracted on: April 29, 2016 (3:25:49 PM)

Consumer Price Index - All Urban Consumers

12-Month Percent Change

Series Id: CUUR0000SA0
 Not Seasonally Adjusted
 Area: U.S. city average
 Item: All items
 Base Period: 1982-84=100



Download: [xlsx](#)

Year	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	HALF1	HALF2
2006	4.0	3.6	3.4	3.5	4.2	4.3	4.1	3.8	2.1	1.3	2.0	2.5	3.8	2.6
2007	2.1	2.4	2.8	2.6	2.7	2.7	2.4	2.0	2.8	3.5	4.3	4.1	2.5	3.1
2008	4.3	4.0	4.0	3.9	4.2	5.0	5.6	5.4	4.9	3.7	1.1	0.1	4.2	3.4
2009	0.0	0.2	-0.4	-0.7	-1.3	-1.4	-2.1	-1.5	-1.3	-0.2	1.8	2.7	-0.6	-0.1
2010	2.6	2.1	2.3	2.2	2.0	1.1	1.2	1.1	1.1	1.2	1.1	1.5	2.1	1.2
2011	1.6	2.1	2.7	3.2	3.6	3.6	3.6	3.8	3.9	3.5	3.4	3.0	2.8	3.5
2012	2.9	2.9	2.7	2.3	1.7	1.7	1.4	1.7	2.0	2.2	1.8	1.7	2.3	1.8
2013	1.6	2.0	1.5	1.1	1.4	1.8	2.0	1.5	1.2	1.0	1.2	1.5	1.5	1.4
2014	1.6	1.1	1.5	2.0	2.1	2.1	2.0	1.7	1.7	1.7	1.3	0.8	1.7	1.5
2015	-0.1	0.0	-0.1	-0.2	0.0	0.1	0.2	0.2	0.0	0.2	0.5	0.7	-0.1	0.3
2016	1.4	1.0	0.9											

Resolution
 Ordinance
 Contract
 Other (Specify) _____

Funding Source _____

APPROVED FOR SUBMITTAL _____

City Manager

STAFF RECOMMENDATION: Approve 2.95% for FY17

Police Dept		Current	Proposed
Staples, Greg	Chief	92,456	95,183
Westvold, Kirk	Asst	78,707	81,029
Downing, Ron	Sgt	74,797	76,293
Martin, Trevor	Sgt	74,797	76,293
Criswell, Brad	Officer	68,515	70,536
Albers, Rafe	Officer	62,525	64,369
Hutchinson, Randy	Officer	62,525	64,369
Lewiston, Ben	Officer	62,525	64,369
Palmer, Steve	Officer	62,525	64,369
Parker, Phil	Officer	62,525	64,369
Spurr, Jason	Officer	62,525	64,369
Bryant, Greg	Officer	56,888	58,566
Hepperly, Greg	Officer	55,016	56,639
Dunlop, Lillian	Officer	50,024	51,500
Croat, Peggy	Clerk	48,194	49,615
Fire/EMS			
Coburn, Ryan	Chief	69,992	72,057
Severance thru Feb	Asst	61,200	63,005
Vetterick, Jennifer	EMS	50,003	51,478
Darst, Chet	EMS	44,990	46,318
Library			
Sealine, Holly	Dir	64,022	65,911
Clark, Annette	Asst	51,002	52,506
Inman, Mallory	Youth	36,046	37,110
P&R			
Kuehl, Nancy	Dir	72,509	74,648
Seibert, Adam	Complex	37,794	38,909
George, Jeff	Rec/Pool	37,794	38,909
Taylor-Seibert, Lou	Admin	38,750	39,894
Administration			
Furler, Jean	FD	112,000	115,304
Eddelman, Jodi	CC	55,494	57,131
Loffredo, Jamie	DCC	40,997	42,206
Snyder, Jo	UC	47,299	48,695
Hock, Amy	UC	38,750	39,894
Comm Develop			
Wagoner, Wade	Dir	77,168	79,444
Stravers, Anthony	Bldg Off	69,493	71,543
Parris, Luke	Planner	60,923	62,720
Campbell, Chris	Inspector	63,253	65,119
Stravers, Shelley	Clerk	42,182	43,427
Staples, Laura	PT Admin		12.35/hour
Public Works			
Hoskins, Tim	Dir	78,707	81,029
Ballard, Chris	Asst	60,000	61,770
Harding, Terry	Mech	51,376	52,892
Baker, Jeremy	PW II	51,376	52,892
Lehmer, Gary	PW II	51,376	52,892
Murillo, Chris	PW II	51,376	52,892
Pettit, Brian	PW II	51,376	52,892
Swift, Paul	PW II	51,376	52,892
Zrucky, Seth	PW II	51,376	52,892
Waugh, Alan	PW II	51,376	52,892
Benge, Mark	PW II	47,029	48,416
Ries, Cody	Laborer	41,101	42,313
Mike, Myer	Lab 2	41,101	42,313
Schultz, Mike	Lab 2	41,101	42,313



**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 19
For Meeting of 05.05.2016

ITEM TITLE: Consideration of a resolution prohibiting tobacco use in the City parks.

CONTACT PERSON: Nancy Kuehl, Parks and Recreation Director

SUMMARY EXPLANATION:

Following is a summary of the proposed resolution establishing a tobacco-free park policy for the City of Norwalk, IA.

Tobacco-free means any product made or derived from tobacco that is intended for human consumption, including any component, part, or accessory of a tobacco product. This includes, among other products, cigarettes, electronic smoking devices, cigarette tobacco, roll-your-own tobacco, smokeless, and dissolvable tobacco.

Areas included in this policy include seating areas of outdoor sports arenas, grounds of any public buildings owned or under the control of a city, playgrounds, athletic fields, skate park, aquatic areas, shelters, restrooms, parking lots, public parks and trails used in the presence of and in proximity to children and adults engaging in or observing outdoor recreational activities, and recreational facilities at all times.

The American Lung Association will be providing appropriate signs to be posted. We are asking city officials, employees, parents, coaches and park users to help enforce this policy. Any person found violating this policy will be asked to cease the use of tobacco or leave the premises.

<p><input checked="" type="checkbox"/> Resolution <input type="checkbox"/> Ordinance <input type="checkbox"/> Contract <input type="checkbox"/> Other (Specify) _____</p> <p>Funding Source: _____</p> <p>APPROVED FOR SUBMITTAL _____</p>

STAFF RECOMMENDATION: This Resolution shall take effect immediately upon its passage.

RESOLUTION NO

A RESOLUTION ESTABLISHING A TOBACCO-FREE PARK POLICY FOR THE CITY OF NORWALK, IOWA

WHEREAS, Section 142D.3 of the Code of Iowa (the Smokefree Air Act) prohibits smoking in public places, including the following outdoor areas: (a) the seating areas of outdoor sports arenas and (b) the grounds of any public buildings owned or under the control of a city; and

WHEREAS, Section 142D.4 of the Code of Iowa establishes certain areas where smoking is not regulated, such as outdoor areas that are places of employment, except where smoking is prohibited by Section 142D.3; and

WHEREAS, Section 142D.5 of the Code of Iowa permits anyone having custody or control of an area otherwise exempt from the smoking prohibitions to declare the entire area as a smoke-free and tobacco-free place; and

WHEREAS, the City of Norwalk, Iowa, maintains public parks and trails for the use and enjoyment of its citizens, their families and people who visit our community. Tobacco use in the presence of and in proximity to children and adults engaging in or observing outdoor recreational activities is detrimental to their health and serves to diminish the enjoyment of using such grounds; and

WHEREAS, the City of Norwalk has a unique opportunity to create and sustain an environment that supports a non-tobacco norm through a tobacco-free policy and rule enforcement; and adult-peer modeling on City owned park, trail and facility grounds; and

WHEREAS, cigarettes and tobacco products, once consumed in public places, are often discarded on the ground requiring additional maintenance expenses, diminish the beauty of the City's recreational facilities, and pose a risk to toddlers due to ingestions; and

WHEREAS, the City of Norwalk determines that the prohibition of tobacco use at the City of Norwalk recreational facilities serves to protect the health, safety and welfare of the citizens and visitors of Norwalk; and

THEREFORE, BE IT RESOLVED that pursuant to the authority granted by Section 142D.5 of the Code of Iowa, all tobacco use is prohibited in outdoor recreational facilities at all times. No person shall use any form of tobacco at or on any City of Norwalk-owned or operated outdoor recreation facilities, including, but is not limited to, any park, playground, athletic field and complex, skate park, aquatic areas, shelters, restrooms, trails and parking lot areas.

Definitions:

Electronic Smoking Devices. The term "electronic smoking device" means any device that can be used to deliver an aerosolized solution that may or may not contain nicotine to the person inhaling from the device, including, but not limited to, an e-cigarette, e-cigar, e-pipe, vape pen, e-hookah or other simulated smoking device.

Tobacco. The term "tobacco" means any product made or derived from tobacco that is intended for human consumption, including any component, part, or accessory of a tobacco product. This includes, among other products, cigarettes, electronic smoking devices, cigarette tobacco, roll-your-own tobacco, smokeless, and dissolvable tobacco. Tobacco product does not include nicotine products approved by the U.S. Food and Drug Administration (FDA) for tobacco cessation.

Enforcement:

- (a) Appropriate signs shall be posted in the above specified areas.
- (b) The community, especially park and facility users and staff, will be notified about this policy.
- (c) City officials, City employees, parents, coaches and park users are asked to help enforce the compliance of this policy.
- (d) Any person found violating this policy will be asked to cease use of tobacco or leave the city park, trail or facility premises.

Effective Date:

This tobacco free park and trail policy shall be effective immediately upon passage of this resolution on this 5th day of May, 2016.

Tom Phillips, Mayor,
City of Norwalk, IA

ATTEST:

Jodi Eddleman, City Clerk
City of Norwalk, IA

<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Kuhl	—	—
Isley	—	—
Lester	—	—
Livingston	—	—
Riva	—	—



**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 20
For Meeting of 5.05.2016

ITEM TITLE: Consideration and discussion of proposed re-organization of Career and POC (paid on call) fire staff.

CONTACT PERSON: Ryan Coburn, Fire Chief

SUMMARY EXPLANATION

With the recent promotion as Fire Chief, this has vacated the full time Assistant Chief position. During the period of having an Interim Chief, and being short positions within the department, the two career staff members Jennifer Vetterick (Fire Medic) and Chet Darst (Fire Medic) have assumed many responsibilities. These include but are not limited to assuming the lead on Rental and Commercial Inspection Programs, training, and EMS administrative duties. In the past, there has been no option for advancement for the career staff which has led to turnover as people are leaving for leadership positions in other departments. The two positions of Firefighter/Paramedic have, for many years, assumed more responsibilities than that of just responding to calls. It would be my recommendation to promote both current staff members to the title of Captain, divide responsibilities into two respective areas, along with a \$5,000. base pay increase to each individual. The funding for these promotions would come from the vacated Assistant Chief position, and the remainder of the salary would be used to fund the salary of an FTE Fire Medic, or a full time Paramedic Firefighter. This would not impact the current approved budget.

Training Captain: Chet Darst- Chet would be responsible for the coordination of monthly training opportunities within the department. This position would also be responsible for physical agility testing of the membership on an annual basis, as well as medical physical coordination. Chet's duties would also include responsibility of Rental and Commercial Inspection Programs which he currently oversees.

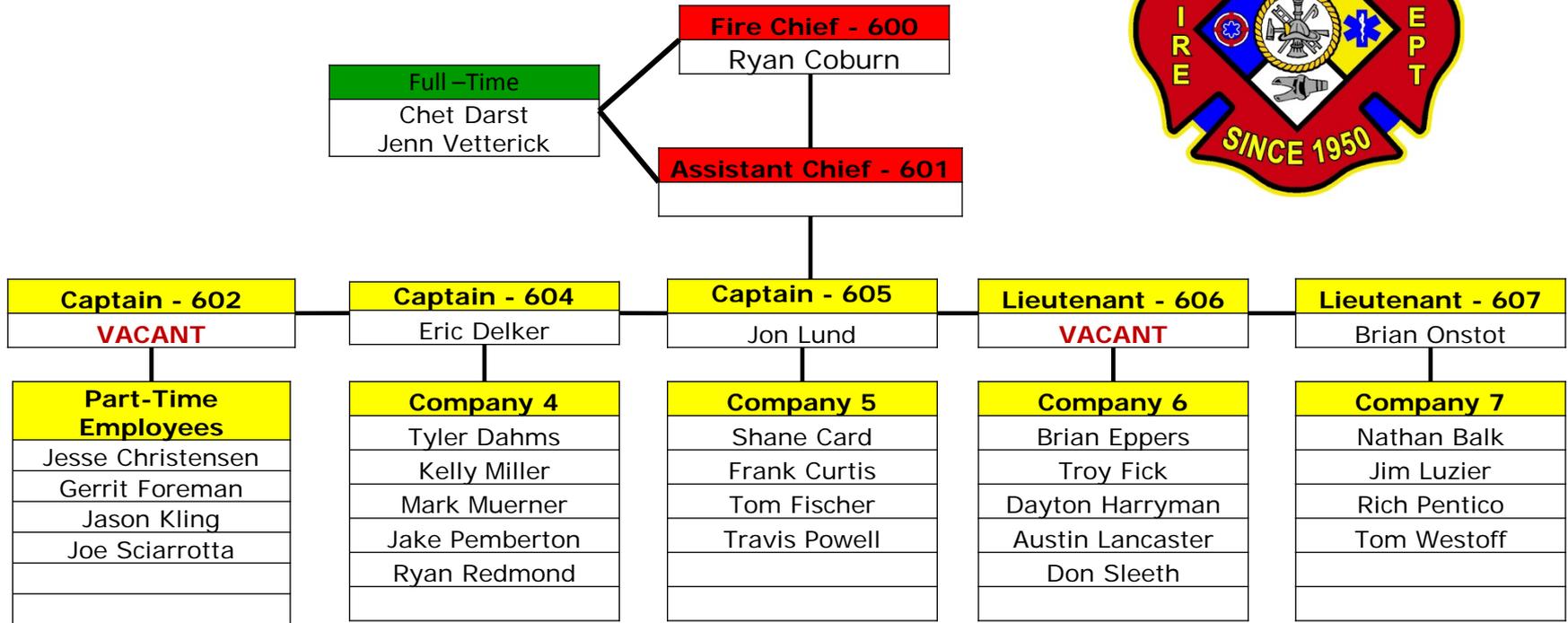
EMS Captain: Jennifer Vetterick – Jenn would be responsible for the daily operations of the EMS division, including oversight of Narcotics and inventory control. This position would also oversee the management of protocols, and policies respective to EMS. Jenn would oversee the ambulance billing and collections, as well as records management.

Lieutenant Brian Onstot- Lt. Onstot would be promoted to the level of Captain after spending two years as a Lt. He is currently filling the role of Captain as a Duty Officer. This promotion would have no impact on the current budget.

The above changes would restore balance to the chain of command in our current departmental structure as shown on the included documents.

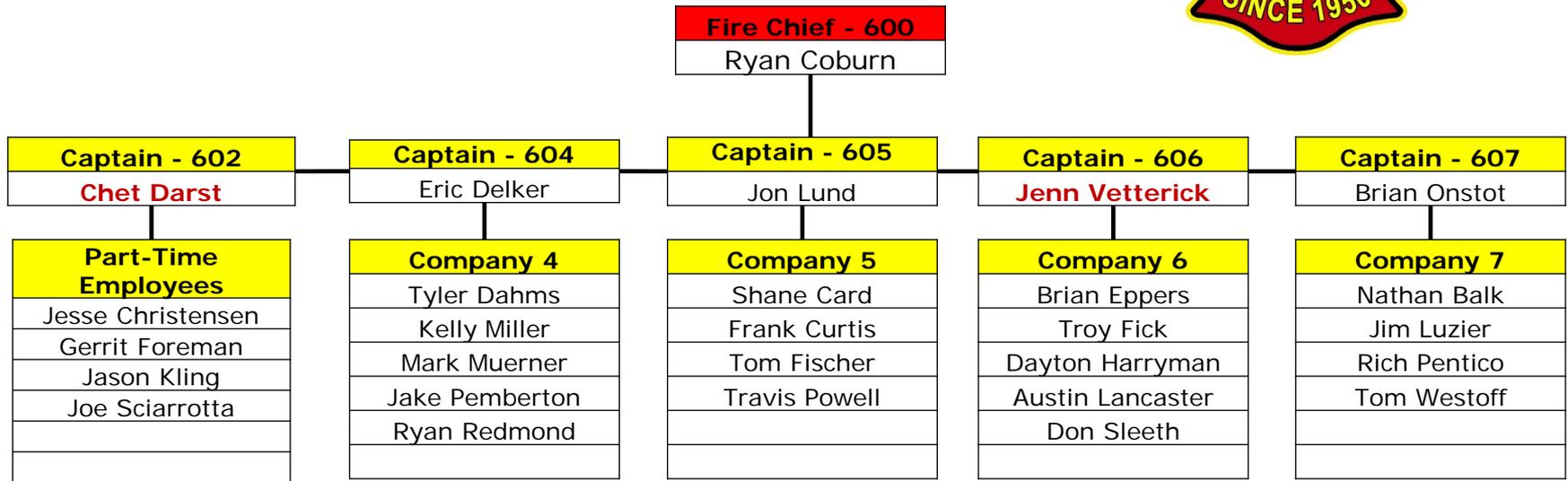
Norwalk Fire Department Organizational Chart

April 2016



Norwalk Fire Department Organizational Chart

April 2016





**BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT**

Item No. 6e
For Meeting of 05.05.2016

ITEM TITLE: FY2015 Audit Presentation and Overview

CONTACT PERSON: Jean Furler, Finance Director

SUMMARY EXPLANATION: The fiscal year 2014-2015 audit was performed by Shull & Co. The following is an overview of the audit:

- The city utilizes cash basis accounting. Governmental Activities include public safety, public works, health & social services, culture & recreation, community & economic development, debt service and capital projects. Business type activities or enterprise funds include water, sewer and storm water utility
- The General Fund balance increased \$577,441 to \$1,584,886 from last fiscal year. Bond Rating companies give considerable weight to a city's general fund balance when determining credit rating.
- The city increased fund balances in every program area with the exception of debt service. Transfers are scheduled in FY16 and FY17 to address the debt service deficit.
- The city's general obligation debt of \$17,358,000 continues to be well below the 5% constitutional debt limit of approximately \$30.2 million.
- Page 67-69 discusses findings for internal control deficiencies and statutory reporting requirements. The city responded to each of these findings and the auditor accepted the responses.
- Page 69 shows deficit balances and how we have addressed those with the exception of the Norwalk Fiber Optic Project. The deficit balance of \$375,227 needs to be eliminated and my goal is to get this done by June 2018 with transfers the next three fiscal years.
- Page 12 Debt Administration section is incorrect. The numbers were not updated from FY14 and should state:
" At June 30, 2015, the City had \$18,662,272 in bonds and other long-term debt, compared to \$15,816,922 last year, as show below."

On that same page Colonial Meadows was listed in FY14 but not FY15 and Water Service Agreement added in FY15. In the course of finalizing the 2014 audit there was discussion of the various development agreement commitments. The Colonial Meadows agreement was subsequently found to require annual appropriation and therefore was no longer considered to be outstanding debt (like most all development agreements). However, further review of the agreement with the Des Moines Waterworks revealed that agreement should be considered a debt obligation. Those items were corrected in the 2015 presentation.

In a set of cash basis statements these items do not affect the cash activity of a given year and are therefore treated as disclosure items so there are no restatements or other reporting issues to deal with only corrections going forward.

Arlen Schrum will be present to answer any other questions you may have.

<p>___ Resolution ___ Ordinance ___ Contract ___ Other (Specify) <u>Simple Motion</u></p> <p>Funding Source: _____</p> <p>APPROVED FOR SUBMITTAL _____</p>
--

STAFF RECOMMENDATION: Approve the FY15 Audit as revised.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS

City of Norwalk, Iowa provides this Management's Discussion and Analysis of its financial statements. This narrative overview and analysis of the financial activities is for the fiscal year ended June 30, 2015. We encourage readers to consider this information in conjunction with the City's financial statements, which follow.

2015 FINANCIAL HIGHLIGHTS

Receipts of the City's governmental activities increased 31%, or approximately \$3,631,000 from fiscal 2014 to fiscal 2015. Property tax and bond proceeds increased approximately \$240,000 and \$5,131,422, respectively. Charges for service, capital grants, contributions and restricted interest, and other general receipts decreased approximately \$116,000, \$964,000 and \$622,000, respectively.

Disbursements of the City's governmental activities decreased 1%, or approximately \$109,000, in fiscal 2015 from fiscal 2014. Debt service and capital projects increased approximately \$755,000 and \$474,000, respectively. Public works, public safety and community and economic development disbursements decreased approximately \$278,000, \$307,000 and \$668,000, respectively.

The City's total cash basis net position increased 203%, or approximately \$5,951,000, from June 30, 2014 to June 30, 2015. Of this amount, the assets of the governmental activities increased approximately \$5,410,000 and the assets of the business type activities increased by approximately \$204,000.

USING THIS ANNUAL REPORT

The annual report consists of a series of financial statements and other information as follows:

Management's Discussion and Analysis introduces the basic financial statements and provides an analytical overview of the City's financial activities.

The Government-wide Financial Statement consists of a Cash Basis Statement of Activities and Net Position. This statement provides information about the activities of the City as a whole and presents an overall view of the City's finances.

The Fund Financial Statements tell how governmental services were financed in the short term as well as what remains for future spending. Fund financial statements report the City's operations in more detail than the government-wide statement by providing information about the most significant funds.

Notes to Financial Statements provide additional information essential to a full understanding of the data provided in the basic financial statements.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

Other Information further explains and supports the financial statements with a comparison of the City's budget for the year and the City's proportionate share of the net pension liability and related contributions.

Supplementary Information provides detailed information about the nonmajor governmental funds and the City's indebtedness.

BASIS OF ACCOUNTING

The City maintains its financial records on the basis of cash receipts and disbursements and the financial statements of the City are prepared on that basis. The cash basis of accounting does not give effect to accounts receivable, accounts payable and accrued items. Accordingly, the financial statements do not present financial position and results of operations of the funds in accordance with accounting principles generally accepted in the United States of America. Therefore, when reviewing the financial information and discussion within this annual report, the reader should keep in mind the limitations resulting from the use of the cash basis of accounting.

REPORTING THE CITY'S FINANCIAL ACTIVITIES

Government-wide Financial Statement

One of the most important questions asked about the City's finances is, "Is the City as a whole better off or worse off as a result of the year's activities?" The Statement of Activities and Net Position reports information which helps answer this question.

The Statement of Activities and Net Position presents the City's net position. Over time, increases or decreases in the City's net position may serve as a useful indicator of whether the financial position of the City is improving or deteriorating.

The Cash Basis Statement of Activities and Net Position is divided into two kinds of activities:

Governmental Activities include public safety, public works, culture and recreation, community and economic development, general government, debt service and capital projects. Property tax and state and federal grants finance most of these activities.

Business Type Activities include the waterworks, the sanitary sewer system, and the storm water drainage system. These activities are financed primarily by user charges.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

Fund Financial Statements

The City has two kinds of funds:

1. Governmental funds account for most of the City's basic services. These focus on how money flows into and out of those funds, and the balances at year-end that are available for spending. The governmental funds include: 1) the General Fund, 2) the Special Revenue funds, such as Road Use Tax and Urban Renewal Tax Increment, 3) the Debt Service Fund and 4) the Capital Projects Funds. The governmental fund financial statements provide a detailed, short-term view of the City's general government operations and the basic services it provides. Governmental fund information helps determine whether there are more or fewer financial resources that can be spent in the near future to finance the City's programs.

The required financial statement for governmental funds is a Statement of Cash Receipts, Disbursements and Changes in Cash Balances.

2. Proprietary funds account for the City's Enterprise Funds and for the Internal Service Fund. Enterprise Funds are used to report business type activities. The City maintains three Enterprise Funds to provide separate information for the water, sewer, and storm water funds. Internal Service Funds are an accounting device used to accumulate and allocate costs internally among the City's various functions.

The required financial statement for proprietary funds is a Statement of Cash Receipts, Disbursements and Changes in Cash Balances.

GOVERNMENT-WIDE FINANCIAL ANALYSIS

Net position may serve over time as a useful indicator of financial position. The City's cash balance for governmental activities increased dramatically from a year ago primarily due to bond proceeds not yet spent, increasing from \$2,646,369 to \$8,070,360. The analysis that follows focuses on the changes in cash balances for governmental activities.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

Changes in Cash Basis Net Position of Governmental Activities		
	Year ended June 30,	
	2015	2014
Receipts and transfers		
Program receipts		
Charges for service	\$ 1,335,294	1,451,423
Operating grants, contributions and restricted interest	1,294,294	1,313,522
Capital grants, contributions and restricted interest	87,975	1,051,834
General receipts		
Property tax	6,986,792	6,747,218
Utility tax replacement excise tax	301,237	293,253
Grants and contributions not restricted to specific purposes	22,782	50,761
Unrestricted interest on investments	4,198	2,568
Bond and loan proceeds	5,131,422	-
Other general receipts	78,954	701,284
Total receipts and transfers	<u>15,242,948</u>	<u>11,611,863</u>
Disbursements		
Public safety	2,601,392	2,879,292
Public works	1,427,091	1,734,437
Health and social services	7,000	9,464
Culture and recreation	1,062,658	1,063,487
Community and economic development	1,064,191	1,732,115
General government	596,861	678,482
Debt service	2,776,005	2,021,490
Capital projects	838,975	364,743
Total disbursements	<u>10,374,173</u>	<u>10,483,510</u>
Change in cash basis net position before transfers	4,868,775	1,128,353
Transfers, net	<u>540,781</u>	<u>46,800</u>
Change in cash basis net position	5,409,556	1,175,153
Cash basis net position beginning of year	<u>2,660,803</u>	<u>1,485,650</u>
Cash basis net position end of year	<u>\$ 8,070,359</u>	<u>2,660,803</u>

The City's total receipts for governmental activities increased by 31%, or \$3,631,085. The total cost of all programs and services decreased by \$109,338, or 1%, with no new programs added this year. The increase in receipts was primarily the result of increases in bond proceeds of \$5,131,422.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

The cost of all governmental activities this year was \$10,374,173 compared to \$10,483,510 last year. However, as shown in the Statement of Activities and Net Position on pages 15-18, the amount taxpayers ultimately financed for these activities was only \$7,656,609 because some of the cost was paid by those directly benefited from the programs (\$1,335,294) or by other governments and organizations that subsidized certain programs with grants, contributions and restricted interest (\$1,382,269). The City paid for the remaining "public benefit" portion of governmental activities with approximately \$6,987,000 in tax (some of which could only be used for certain programs) and with other receipts, such as interest and general.

Changes in Cash Basis Net Position of Business Type Activities			
	Year ended June 30,		
	2015		2014
Receipts and transfers			
Program receipts			
Charges for service			
Water	\$ 1,388,767		1,276,466
Sewer	1,784,550		1,604,259
Other non major	354,392		345,569
Operating grants, contributions and restricted interest	161,527		132,411
General receipts			
Unrestricted interest on investment	4,562		5,335
Bond proceeds	<u>425,000</u>		<u>-</u>
Total receipts	<u>4,118,798</u>		<u>3,364,040</u>
Disbursements			
Water	1,179,914		1,382,563
Sewer	1,961,068		1,462,847
Other non major	<u>233,472</u>		<u>163,937</u>
Total disbursements and transfers	<u>3,374,454</u>		<u>3,009,347</u>
Change in cash basis net position before transfers	744,344		354,693
Transfers, net	<u>(540,781)</u>		<u>(46,800)</u>
Change in cash basis net position	203,563		307,893
Cash basis net position beginning of year	<u>3,290,133</u>		<u>2,982,240</u>
Cash basis net position end of year	<u>\$ 3,493,696</u>		<u>3,290,133</u>

Total business type activities receipts for the fiscal year were \$4,118,798 compared to \$3,364,040 last year. Total disbursements for the fiscal year increased by 12% or a total of

\$365,107.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

INDIVIDUAL MAJOR GOVERNMENTAL FUND ANALYSIS

As City of Norwalk, Iowa completed the year, its governmental funds reported a combined fund balance of \$8,070,360, an increase of more than \$5,410,000 from last year's total of \$2,660,803. The following are the major reasons for the changes in fund balances of the major funds from the prior year.

The General Fund cash balance increased \$577,441 from the prior year to \$1,584,886.

The Road Use Tax Fund cash balance increased by \$147,092 during the fiscal year to \$613,699.

The Employee Benefits Levy Fund cash balance increased \$160,925 to \$826,165.

The Urban Renewal Tax Increment Fund cash balance was \$2,415,530, an increase of \$823,422 from the previous year.

The Debt Service Fund cash balance was negative \$221,781, a decrease of \$116,906 from the previous year.

The combined Capital Projects Funds increased \$3,378,253 from the previous year.

INDIVIDUAL MAJOR BUSINESS TYPE FUND ANALYSIS

The Water Fund cash balance increased by \$313,213 to \$1,195,925.

The Sewer Fund cash balance increased by \$222,022 to \$1,193,109.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

BUDGETARY HIGHLIGHTS

Over the course of the year, the City amended its budget one time. The amendment was approved on May 21, 2015 and resulted in a net increase of \$4,708,255 in revenues. The increase was due primarily to a planned sale of land. Budgeted expenditures increased \$247,575 due to capital purchases.

Even with the budget amendments, the City exceeded the amounts budgeted in the debt service function for the year ended June 30, 2015.

DEBT ADMINISTRATION

At June 30, 2015, the City had \$18,662,272 in bonds and other long-term debt, compared to \$15,816,922 last year, as show below.

Outstanding Debt at Year-End			
	June 30,		
	2015		2014
General obligation bonds	\$ 17,185,000		14,640,000
General obligation capital loan notes	172,500		-
Revenue bonds	756,500		441,000
Sanitary district mortgage assumed	106,540		122,060
Water service agreement	412,357		565,336
Equipment loans	29,375		48,526
Total	<u>\$ 18,662,272</u>		<u>15,816,922</u>

The Constitution of the State of Iowa limits the amount of general obligation debt cities can issue to 5% of the assessed value of all taxable property within the City's corporate limits. The City's outstanding general obligation debt of \$17,358,000 is significantly below its constitutional debt limit of approximately \$30 million.

More detailed information about the City's long-term debt is presented in Note 3 to the financial statements.

CITY OF NORWALK

MANAGEMENT'S DISCUSSION AND ANALYSIS (Continued)

ECONOMIC FACTORS AND NEXT YEAR'S BUDGETS AND RATES

City of Norwalk, Iowa's elected and appointed officials and citizens considered many factors when setting the fiscal year 2016 budget, tax rates, and fees that will be charged for various City activities. One of those factors is the economy. The City's total assessed valuations have increased slightly. However, funding from the State has decreased due to budget constraints.

CONTACTING THE CITY'S FINANCIAL MANAGEMENT

This financial report is designed to provide our citizens, taxpayers, customers, and creditors with a general overview of the City's finances and to show the City's accountability for the money it receives. If you have questions about this report or need additional financial information, contact Marketa George Oliver, City Manager, 705 North Avenue, Norwalk, Iowa.



BUSINESS OF THE CITY COUNCIL
AGENDA STATEMENT

Item No. 14
 For Meeting of 05.05.16

- REQUEST:** Consideration of second and possible third reading of an ordinance amending the master plan and rules, regulation and guidelines for the Dobson Planned Unit Development as contained in Ordinance No.15-05
- STAFF CONTACT:** Luke Parris, AICP
 City Planner
- APPLICANT(S):** Cort Landing LLC
 340 Wright Road Suite E
 Norwalk, Iowa 50211
- LOCATION:** Southwest intersection of Iowa Highway 28 and Elm Avenue.
- CURRENT USE:** C-2 Commercial.
- PROPOSED USE:** R-1(60) Residential.
- ZONING HISTORY:** The site is zoned as Parcel D of the Dobson Planned Unit Development with a classification of "C-2" Community Commercial. This area has been zoned C-2 since the July 15, 2004 adoption of the Dobson PUD (Ordinance No. 04-08) and the amendment to the Dobson PUD (Ordinance No. 15-05) on June 4, 2015.
- LAND USE PLAN:** The future land use plan designates this location medium density residential.
- SURROUNDING LAND USE PLAN AND ZONING:** Surrounding land use planned for the area is:
- North - Medium Density Residential.
 - East - Medium Density Residential.
 - South - Medium Density Residential.
 - West - Medium Density Residential.
- Surrounding zoning for the area is:
- North - "R-1" Residential.
 - East - "R-1" Residential.
 - South - "R-1" Residential.
 - West - "R-1" Residential.

FLOOD INFORMATION:

None.

**MAJOR STREET
PLAN/TRAFFIC:**

The request would not appear to have a negative impact on traffic conditions. Vehicles would access the lots from the extension of Pine Avenue as shown on the preliminary plat for Cort Landing. All roads are classified as local streets.

**DEVELOPMENTSECTOR
ANALYSIS:**

Parcel D is currently a C-2 area with frontage along Iowa Highway 28. While Iowa Highway 28 frontage exists, access to Iowa Highway 28 from the site is unlikely to be permitted by the Iowa DOT. Access would be off of the internal street, Pine Avenue. The commercial site is adjacent to existing and planned single family homes on all sides.

STAFF ANALYSIS:

The parcel is currently zoned C-2 and fronts along Iowa Highway 28. The parcel is only undeveloped C-2 parcel south of North Avenue. The majority of neighboring property is zoned for single family residential with some industrial ground farther to the south along Iowa Highway 28.

The future land use plan for the area is identified as Medium Density Residential. In these areas, the Comprehensive Plan does not call for any commercial type uses.

There is a conflict between the current zoning and the future land use plan. In the recent PUD amendment, the parcel retained the C-2 zoning because the initial PUD had been approved prior to the 2013 Comprehensive Plan. Rezoning the parcel to R-1(60) would more closely match the future land use plan approved in the 2013 Comprehensive Plan.

The developer is also concerned about the developability of the parcel for C-2 uses. The concern is mainly due to a PUD requirement that the site has a 30' buffer adjacent to any single family uses. A staff analysis determined that the building envelope could likely fit a commercial building but that the buffer requirement hampers the ability to provide appropriate parking on the site. To approve a commercial site plan for the site, the City would need to be willing to reduce the 30' buffer requirement in the PUD.

**STAFF
RECOMMENDATION:**

The rezone proposal to R-1(60) is in accordance with the future land use identified in the Comprehensive Plan and would be cohesive with the existing uses and zoning that is already single family residential.

Staff recognizes the potential difficulties in developing the parcel as a commercial site due to buffer requirements. Additionally, staff also recognizes that the City has limited commercial ground along Iowa Highway 28 and that the proposal would reduce that number further.

Staff recommends that consideration be given to the developability of the parcel, the loss of commercial ground, and the context of the existing land uses nearby when making a decision on the proposal.

**PLANNING AND ZONING
RECOMMENDATION:**

The Planning and Zoning Commission recommends approval of the request to rezone Parcel D of the Dobson PUD from C-2 to R-1 (60).

ATTACHMENTS:

Attachment "A" – Dobson PUD Parcel D Rezoning Map
Attachment "B" – C-2 Parcel Maps with Building & Parking
Envelopes
Attachment "C" – Ord. 15-05 – Dobson PUD Amendment

____Resolution	<input checked="" type="checkbox"/> Ordinance	____Contract	____Other (Specify)
Funding Source: ____ NA			
APPROVED FOR SUBMITTAL _____			
City Manager			

ORDINANCE NO. _____

**AN ORDINANCE AMENDING THE MASTER PLAN AND RULES, REGULATION,
AND GUIDELINES FOR THE DOBSON PLANNED UNIT DEVELOPMENT AS
CONTAINED IN ORDINANCE NO. 15-05**

BE IT ENACTED BY THE CITY COUNCIL OF THE CITY OF NORWALK, IOWA:

SECTION 1. PURPOSE. The purpose of this ordinance is to amend the master plan and rules, regulation, and guidelines for Dobson Planned Unit Development as contained in Ordinance No. 15-05.

SECTION 2. AMENDMENT. The Dobson Planned Unit Development is hereby amended with the following:

Section 5: Land Use Design Criteria:

Amend the land use and density schedule for Parcel D, as shown below:

LAND USE AND DENSITY SCHEDULE					
Parcel #	Land Use/ Zoning	Max Density	Area/Acres	# Units	Density DU/Acre
Parcel D	R-1(60)	4 DU/Acre	2.3	9	3.9

Amend the bulk regulations for Parcel D and add Parcel E, as shown below:

BULK REGULATIONS						
Parcel #	Lot Area	Lot Width	Yard			Height
	Minimum Sq. Ft.	Feet	Front Feet	Side Feet	Rear Feet	Feet
Parcel D	7,500	60'	30'	15' total (min. 7' one side)	35'	35'

Under "Specific Information Not In Tables" remove Parcel D and replace with the following:
"Parcel D. A public street meeting City standards is required to connect Elm Avenue to Parcel E to the south and to Outlot Y of Arbor Glynn Plat 2.

SECTION 3. SEVERABILITY CLAUSE. In any section, provision, or part of this ordinance shall be adjudged invalid or unconstitutional such adjudication shall not affect the validity of the ordinance as a whole or any section, provision or part thereof not adjudged invalid or unconstitutional.

SECTION 4. EFFECTIVE DATE. This Ordinance shall be in full force and effect after its passage, approval and publication as provided by law.

Passed and approved by the City Council of the City of Norwalk, Iowa on the _____ day of _____, 2016.

Tom Phillips, Mayor

ATTEST:

Jodi Eddleman, City Clerk

PREPARED BY: Luke Parris, City Planner

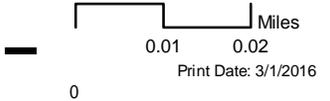
<u>ROLL CALL VOTE:</u>	<u>Aye</u>	<u>Nay</u>
Isley	_____	_____
Kuhl		
Lester		
Livingston		
Riva	_____	



Source: Esri, DigitalGlobe, GeoEye, Earthstar, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



CORT LANDING LLC REZONE REQUEST MAP



ELMAVE

203'

123'

PARKING

83'

BUILDING ENVELOPE

483'

412'

376'

387'

427'

507'

SUNSET DR

77'

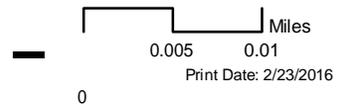
110'

186'

Source: Esri, DigitalGlobe, GeoEye, Earthstar, USDA, USGS, AEX, Geomatics, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



Dobson PUD C-2 Parcel Building & Parking



ELMAVE

203'

30 FOOT BUFFERYARD

10 FOOT PARKING SETBACK

123'

483'

412'

PARKING ENVELOPE

427'

507'

SUNSET DR.

110'

186'

Source: Esri, DigitalGlobe, GeoEye, Earthstar, USDA, USGS, AEX, Geomapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

Dobson PUD C-2 Parcel Parking Envelope

0 0.005 0.01 Miles
 Print Date: 2/23/2016

203'

30 FOOT BUFFERYARD

30 FOOT SETBACK

83'

**BUILDING
ENVELOPE**

483'

376'

387'

507'

SUNSET DR

77'

186'

Source: Esri, DigitalGlobe, GeoEye, i-cubed, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

ELMAVE

200'

30 FOOT BUFFER

Dobson PUM C-2 Parcel Building Envelope

30 FOOT BUFFER

20'

20 FOOT
BUILDING
ENVELOPE

20'

20'

20'

200'

Miles
0.005
0.01
Print Date: 2/23/2016

0

3000' 0"

Source: Esri, DigitalGlobe, GeoEye, I-cubed, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

ORDINANCE NO. 15-05

AN ORDINANCE AMENDING THE MASTER PLAN AND RULES, REGULATIONS AND GUIDELINES FOR THE DOBSON PLANNED UNIT DEVELOPMENT AS CONTAINED IN ORDINANCE NO. 04-08

BE IT ENACTED by the City Council of the City of Norwalk, Iowa.

SECTION 1. PURPOSE. The purpose of this ordinance is to amend the master plan and rules, regulation, and guidelines for Dobson Planned Unit Development as contained in Ordinance No. 04-08.

SECTION 2. AMENDMENT. The Dobson Planned Unit Development is hereby amended with the following:

Include the amended Dobson Planned Unit Development map attached as Exhibit "A".

Section 5: Land Use Design Criteria:

Amend the land use and density schedule for Parcel D and add Parcel E, as shown below:

LAND USE AND DENSITY SCHEDULE						
Parcel#	Land Use/ Zonin	Max Densit	Area/Acres	# Units	Density DU/Acre	
Parcel D	C-2 Commercial	N/A	3.0			
Parcel E	R-1 60	4 DU/Acre	11.4	35	3.1	

Amend the bulk rEill,ulations for Parcel D and add Parcel E as shown below:

<u>r BULK REGULATIONS</u>						
Parcel #	Lot Area Minimum Sc. Ft.	Lot Height Feet	Yard			
			Width Front Feet	Side Feet	Rear Feet	Feet
Parcel D	20,000	100'	30'	20' total (0' side yard for complex	35'	50'
Parcel E	7,500	60'	30'	15' total (min. 7' one s i d e ----- L-----	35'	35'

Under "Specific Information Not In Tables" remove Parcel D and replace with the following:

"Parcel D. Uses in this parcel are limited to C -2 district uses and are encouraged to be lower traffic generating uses due to their proximity to single family residential uses. The parcel is for light intensity Commercial uses described in the C-2 zoning district. A public

street meeting City standards is required to connect Elm Avenue to Parcel E to the south and to Outlot Y of Arbor Glynn Plat 2. Proximity to adjacent single family residential uses will require appropriate buffers. Any part of Parcel D that is adjacent to a single family lot shall be required to have a 30' landscaped buffer that meets the requirements of Chapter 17.50 of the City of Norwalk Zoning Ordinance. All setbacks shall be measured from the landscaped buffer, if required. Access to Parcel D from a public street to the west shall align with the access for Outlot Y of Arbor Glynn Plat 2.

Allowed Uses: All permitted principal and accessory uses and special uses as provided in the City Code for the C-2 district except the following:

- i. Bars and Night Clubs, including after-hour businesses
- ii. Convenience Stores, Gas Stations and Service Stations
- iii. Delayed Deposit Service Businesses
- iv. Liquor Stores
- v. Pawnshops
- vi. Smoking Lounges and Dens
- vii. Tobacco Stores

Under "Specific Information Not In Tables" add the following:

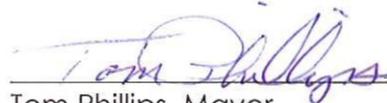
"Parcel E. Access off of Wright Road will not be required due to geographic concerns . Access into the parcel from Iowa Highway 28 shall meet the standards Iowa Department of Transportation. A public street shall provide access to north to Elm Avenue ."

SECTION 3. REPEALER. All ordinances or parts of ordinances in conflict with the provisions of this ordinance are hereby repealed.

SECTION 4. SEVERABILITY CLAUSE. In any section, provision, or part of this ordinance shall be adjudged invalid or unconstitutional such adjudication shall not affect the validity of the ordinance as a whole or any section, provision or part thereof not adjudged invalid or unconstitutional.

SECTION 5. EFFECTIVE DATE. This Ordinance shall be in full force and effect after its passage, approval and publication as provided by law.

Passed and approved by the City Council of the City of Norwalk, Iowa on the 4th day of June, 2015.



Tom Phillips, Mayor

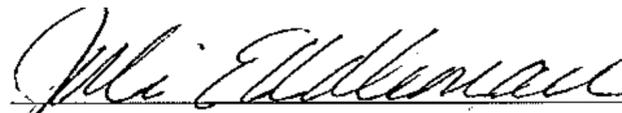
ATTEST:



Jodi Eddleman, City Clerk

f
First Reading: May 21, 2015
Second Reading: June 4, 2015
Third Reading: "w"a,.,_iv"e"d"----

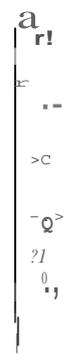
I certify that the foregoing was published as Ordinance No. 15-05 on the 11th day of June, 2015.


Jodi Eddleman, City Clerk

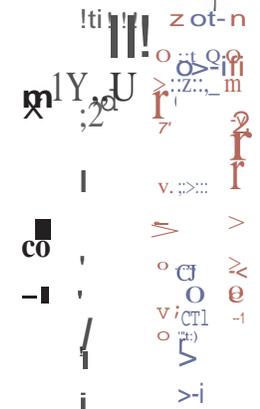


LOT I - OLI: C.HARD RDGE PLAT :3

C.O.-IC.EPTIAL LOT LAYO T - OPTION 'A'



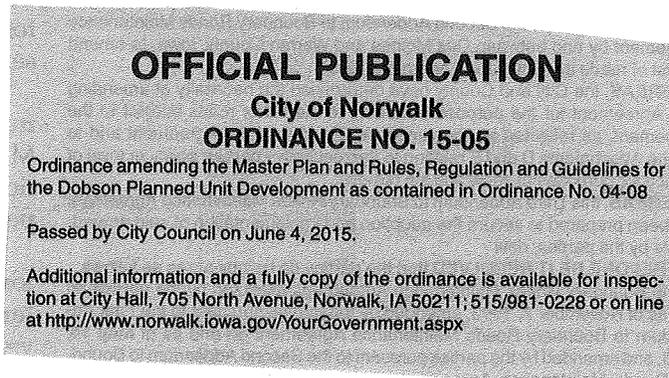
NO.	DATE	REVISIONS	COMMENTS
1	4.6.51 24. 2024		



Cif Engineering Consultants, Inc.

AFFIDAVIT OF PUBLICATION

COPY OF NOTICE



I, Sally M. Huntoon on oath, do say that I am the Editor of the NORTH WARREN TOWN AND COUNTY NEWS, a weekly newspaper of general circulation, published at Norwalk, in Warren County, Iowa and that the attached Official Publication of the City of Norwalk in re: Ordinance 15-05 on Amending Master Plan and Rules, Regulations and Guidelines for Dobson Planned Unit Development and published in the above-named newspaper as follows:

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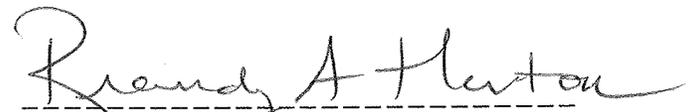
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Sally M. Huntoon

Sworn to before me and signed by
Sally M. Huntoon

Dated this 12th day of June, 2015.


Notary Public in and for Warren County Iowa

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